

# Appendix D

## Public Engagement

- Appendix D.1 – *Proposed Wye Road Plan Functional Planning Study, Stakeholder Information Meeting Input Summary (IMI Strategies) – 2008*
- Appendix D.2 – *Proposed Wye Road Plan Functional Planning Study, May 2008 Public Open House Summary (IMI Strategies) – 2008*
- Appendix D.3 – *Wye Road Stakeholder Engagement Summary Report (Stantec)*
- Appendix D.4 – *Wye Road Functional Planning Study, Public Open House Summary (Al-Terra Engineering) – 2014*



## Appendix D.1

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### Proposed Wye Road Plan Functional Planning Study, Stakeholder Information Meeting Input Summary *(IMI Strategies) – 2008*







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**PROPOSED WYE ROAD PLAN  
~ FUNCTIONAL PLANNING STUDY ~**

**STAKEHOLDER INFORMATION MEETING  
INPUT SUMMARY**

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*prepared for*

**Strathcona County**

April 15, 2008

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## 1. INTRODUCTION

A Stakeholder Information Meeting was held at the Strathcona County Offices on March 19, 2008, for the purpose of presenting the proposed Wye Road Plan (the Functional Planning Study) to stakeholders. The proposed plan was presented by Al-Terra Engineering Ltd. Tenants and landowners/business owners adjacent to Wye Road were invited to attend by a mailed and/or hand-delivered invitation. Close to 200 notices were mailed and/or hand-delivered, as indicated in Appendix C.

This document provides results obtained from Comment Forms made available to stakeholders in attendance. The Comment Form asked stakeholders to indicate their level of agreement with aspects of the proposed plan as well as to provide written comments.

Six completed Comment Forms were received. Section 2 of this document provides input from these six forms, presented first in tabular form for each aspect of the proposed plan addressed on the Comment Form, followed by verbatim written comments associated with the tabular responses. Section 2 also includes feedback on the value of the meeting.

Appendix A contains the Stakeholder Comment Form prepared by IMI *strategics*. Appendix B contains Meeting Minutes. These Minutes were prepared by Al-Terra Engineering Ltd. and are provided as information supplementary to the Comment Form stakeholder input. Appendix C contains the notification list for the meeting as used by Al-Terra Engineering Ltd. to invite stakeholders.



## 2. STAKEHOLDER INPUT

Various statements were provided on the Comment Form about aspects of the Wye Road Plan, to which stakeholders could provide feedback by indicating their level of agreement and providing comments. The responses received using the Comment Form are set out here first in tabular form, and are followed by stakeholder written comments associated with the statement. Note that only six Comment Forms were received, and not all respondents answered every question. Tabular and comment results should, accordingly, be viewed within those constraints.

### 2.1 PROPOSED WYE ROAD PLAN AS A CONCEPT FOR SHERWOOD PARK

Among the six individuals who responded to this statement about whether the proposed Wye Road Plan seems like a good concept for Sherwood Park, one agreed and five disagreed.

<b>Overall, the proposed Wye Road Plan seems like a good concept for Sherwood Park.</b>		
<b>Answer Options</b>	<b>Response Percent</b>	<b>Response Count</b>
Strongly Agree	0%	0
Somewhat Agree	17%	1
Somewhat Disagree	17%	1
Strongly Disagree	66%	4
<i>answered question</i>		<b>6</b>
<i>skipped question</i>		<b>0</b>

#### 2.1.1 Comments on Ideas That Will Work Well

##### *Somewhat Agreed*

- i) Traffic flow.

##### *Somewhat Disagreed*

- i) Wye Road Plan – Q: the overall PROPOSED WYE ROAD PLAN seems like a good concept for Sherwood Park. I have to disagree. Al-Terra's job was to improve traffic flow at several intersections between Highways 21 and 216. I am sure their 'models' are tried and true and will work for the stated purpose in those areas. They look convincing on the simulation presented at the meeting (March 19). However, Strathcona County has missed the point TOTALLY on several critical issues. I drive into the Park almost every day of the year and frequently come in between the times of 7:30 and 8:00am. I am lucky, I admit; I come in on 216 (north) and turn onto Wye Road (east), missing ALL OF THAT TRAFFIC GOING INTO THE CITY. It is common that traffic is moving slow or backed up into the Park from the overpass on 216. This means traffic is backing up from west of the overpass, not necessarily from a few red light delays on Wye Road. In addition, the proposal is to make Wye Road three lanes wide (one way), but



the Sherwood Park Freeway is only TWO lanes wide. Doesn't that mean that you'll just get those people through Sherwood Park and to that bottle-neck quicker? How is that improving things and at what cost?

*Strongly Disagreed*

- i) More lanes east and west will help the traffic flow.
- ii) The additional flow lanes will improve traffic flow during peak PM timeframes, but the elimination of left turning lanes is not necessary.

**2.1.2 Comments on Ideas that Strathcona County Should be Cautious About**

*Somewhat Agreed*

- i) No left turn 24 hours a day on RR 232, only needed at peak time!!

*Strongly Disagreed*

- i) Removing the left turn for west bound traffic on Wye Road on to 233 is going to remove all good access to the businesses and will create monumental problems for commuters, and is removing the existing left turn lane into Wal-Mart for south bound traffic on 233. The left turn into the Rona site off the new "dogleg" road will also cause problems. Not widening Wye Road where it turns left on to the ring road is a mistake.
- ii) There has been no consideration given to the impact on business to the operators on Ordze Ave. The elimination of left turning lanes cannot be allowed to go forward.
- iii) There has been no consideration for the impact on existing businesses.

**2.2 CLOVERBAR ROAD / NOTTINGHAM AREA**

Among the three individuals who responded to the statement about whether the suggestions for the Cloverbar Road/Nottingham area seem like good concepts, one somewhat agreed and two strongly disagreed.

**Overall, the suggestions for the Cloverbar Road/Nottingham area seem like good concepts.**

Answer Options	Response Percent	Response Count
Strongly Agree	0%	0
Somewhat Agree	33%	1
Somewhat Disagree	0%	0
Strongly Disagree	67%	2
<i>answered question</i>		<b>3</b>
<i>skipped question</i>		<b>3</b>

### *Strongly Disagreed*

- i) Poorly thought out based upon "projected" traffic flows. The trans dept is spending the taxpayers money without due considerations. There have been a few complaints about these intersections, they are not dangerous, yet the traffic dept still feels obliged to create a make work project for themselves.
- ii) There has been no consideration for the impact on existing businesses.

## **2.3 BRENTWOOD BOULEVARD INTERSECTION**

Among the four individuals who responded to the statement about whether the suggestions for the Brentwood Boulevard intersection seem like good concepts, all disagreed.

<b>OVERALL, THE SUGGESTIONS FOR THE BRENTWOOD BOULEVARD INTERSECTION SEEM LIKE GOOD CONCEPTS.</b>		
Answer Options	Response Percent	Response Count
Strongly Agree	0.0%	0
Somewhat Agree	0.0%	0
Somewhat Disagree	25.0%	1
Strongly Disagree	75.0%	3
<i>answered question</i>		<b>4</b>
<i>skipped question</i>		<b>2</b>

### *Somewhat Disagree*

- i) Left turn at peak times makes sense. Do a count before mandating a 24 hour no left!!



*Strongly Disagreed*

- i) No doglegs.
- ii) There has been no consideration for the impact on existing businesses.
- iii) We need left turning lanes.

**2.4 SHERWOOD DRIVE / WALLACE DRIVE / ORDZE AVENUE INTERSECTION**

Among the four individuals who responded to this statement about whether the suggestions for the Sherwood Drive / Wallace Drive / Ordze Avenue intersection seem like good concepts, four disagreed.

<b>Overall, the suggestions for the Sherwood Drive / Wallace Drive / Ordze Avenue intersection seem like good concepts.</b>		
<b>Answer Options</b>	<b>Response Percent</b>	<b>Response Count</b>
Strongly Agree	0%	0
Somewhat Agree	0%	0
Somewhat Disagree	0%	0
Strongly Disagree	100%	4
<i>answered question</i>		<b>4</b>
<i>skipped question</i>		<b>2</b>

*Strongly Disagreed*

- i) I've traveled to a lot of places where traffic moves slow during 'rush hour', and that is just the way it is. Three lanes, four lanes, five lanes, not even six lanes improved anything and seemed to cause safety problems when people chose to change lanes 'too late' to reach their exits. I've traveled to places where the overhead signboard says '18 km line-up ahead, choose alternate route'. But apparently in Sherwood Park, waiting through a few red lights at a certain time of day is such a significant inconvenience that a whole upheaval of the road system is required. The new 'jughandle' that goes behind WalMart will significantly affect traffic flowing into the Ordze Avenue area. When the WalMart-Ordze Avenue intersection on Range Road 232 is closed, the Ordze Avenue area will basically cease to exist. Irrespective of this, the 'jughandle' will divert traffic through a new business area. It is unreasonable to think that the general consumer will be interested in accessing an area like Ordze Avenue when access is difficult, compared to the ease of accessing the new businesses along the 'jughandle'. If, or more like when, the second part of the 'jughandle' is completed (through Canadian Tire's and Expert Lock- Brentwood Animal Hospital's parking lots), it is likely that traffic passing along this road will largely be using it to get back onto Wye Road. The businesses (that now FACE Ordze Avenue at this time) immediately along this section of Wallace Drive will be facing the wrong way, including all of the signage and driveways (accesses). Even if the berm is removed, the 'backs' of these businesses were never developed to attract





consumer attention. And Strathcona County itself demanded when these properties were developed that they face Ordze Avenue and present a reasonably attractive frontage.

- ii) The operation was a success but the patient died -- it will save the traffic problem but have a huge negative impact on the businesses located on Ordze Avenue.
- iii) There has been no consideration for the impact on existing businesses.

## **2.5. GENERAL COMMENTS**

The following comments were received about the proposed Wye Road Plan in general.

- i) As a business owner in this area, the prospects are grim. No one has yet pointed out the benefits to me. Al-Terra insists that traffic will be increased. If the second part of the 'jughandle' is built, I still believe that a lot of that traffic will have done their shopping before they reach my location; my area just becomes a by-pass. If the second part of the 'jughandle' is not built, traffic flow into my area is seriously compromised. As a business owner at my current location, I stand to lose even more. The 'new road' (Wallace Drive west of Range Road 232), if (when) built, goes right through the parking lot of this property. This will create innumerable problems for my clients, staff and suppliers. First of all, parking is already limited – the parking lot cannot be enlarged. And under the proposed (probable) development, the Expert Lock–Brentwood Animal Hospital parking lot gets SMALLER. In order to improve visibility and insure safe exit from the parking lot, additional spaces on the west side will have to be given up. Parking in front of my neighbor, Expert Lock, will be severely restricted, meaning their customers will be forced to park in front of my location. I run a Veterinary Hospital. I require excellent access right in front of my building for people with pet carriers, unruly pets, pets with poor mobility and for pets being picked up after surgery. I have that now. I won't have it in the future if (when) this project proceeds. Currently, large delivery vehicles back down (or up) the west side of the building to deliver supplies to the rear of the building. If (when) this project proceeds, how will those trucks exit the rear parking area? They can't be expected to back onto a roadway. Deliveries cannot reasonably be made to the 'front' of the building due to traffic and parking issues there. My business was not designed to handle deliveries to the front of the building. At this time, we store snow on the property over the winter. This is not without challenges. The slope of the berm prohibits much snow storage, so one parking spot on each side of the rear parking area is used for snow storage. The berm faces north, so snow melt in the spring is prolonged – pooling and icing are an issue. When the west part of the rear parking area is sacrificed to the roadway, we will have insufficient parking spaces remaining for the staff from both businesses that operate out of this location. In the winter, this problem will be compounded. As well, in the winter, snow plowed from the new roadway onto the shoulder will, at some time, obstruct the view of those attempting to exit the parking area (front and rear), creating a safety issue. My lease promises me a certain amount of access and use from this property. The property was designed specifically for the two businesses that operate from this location. The County's plans will cause





me inordinate issues in this respect. For me, the meeting (Mar 19) was a formality. Lip-service. An attempt at placation. For me, the Wye Road Plan is a foregone conclusion. And I'm drawing my conclusions accordingly. Loss of adequate visibility, access and functional property will affect not only my 'bottom line', but will affect the overall value of my business for the future. I think that Strathcona County effectively will have (has) destroyed my livelihood. And I don't even have time to react.

- ii) It would appear that new businesses locating on Wye Rod are being accommodated at the expense of existing businesses in the area.
- iii) 1. Traffic consultant stated that changes are required as a result of the "traffic model". There can be many ulterior motives with a model and it is just a projection - it is not reality nor should it be considered an accurate predictor. I have created a lot of econometric models (I have a Master of Science Degree from the Massachusetts Institute of Technology (MIT)) and the result can be altered quite radically by a small shift in assumptions. I request a full copy of the model so that a consultant representing our interests can review its design and assumptions. 2. The traffic consultant stated that the need for changes was caused by the projected increase in traffic caused by forthcoming (again ... projected) developments. Should not the developers be responsible for the costs rather than the taxpayers? Consultant did not know the costs ... did not know the contributions from developers... did not seem concerned about the cost ... only about the intersection. I request a full copy of the budget for the project along with a calculation of the contribution by the developers. 3. Consultant stated that residents of Strathcona County did not put up with traffic congestion, but could not offer any factual evidence that they received an overwhelming number of phone calls (or any phone calls for that matter) regarding the delays at the intersection of Wye Road and 233. I request a form stating the number of complaints received the department in the last year regarding the intersection of Wye Road and 233, as compared to other intersections. 4. Consultant stated that the Wye Road/233 intersection was a level "D" intersection and people in Edmonton would put up with level F intersections. Again, where is the proof the residents of Strathcona County are demanding changes, and should the taxpayers of Strathcona County be required to pay to the requests of a few, if any, impatient people? 5. Consultant did not consider any of the adverse impacts that the changes to the intersection of Wye Road/233 would have on the existing businesses. Can you imagine the City of Edmonton making changes to the Yellowhead without looking at the impacts on business? This is absolutely the worst case I have ever heard of a Town/City neglecting the issues that affect the taxpayers. 6. The mayor/Councilors were not provided notice of this meeting and did not have any representative attend the meeting. I congratulate Councilor Carr who happened to be in the building on other business who made time in her schedule to attend the meeting. 7. The consultant/Trans dept did not make proper minutes at the meeting. I request a copy of the minutes taken as well as a copy of all the comments received. Why was there not an unbiased party at the meeting taking proper minutes? 8. Why does the Rona site have a left-in intersection on Ash Road/Wallace similar to the one into Wal-Mart from 233 that is permitted, but the left in to Wal-Mart is set to be terminated? 9. What are the



legal ramifications of taking away the left-in intersection into the Wal-Mart site off 233 which was provided to Wal-Mart as part of the development permit for the Wal-Mart site? 10. Again, how ridiculous is it that the consultants/trans dept only consideration was the traffic flow through the intersection of Wye Road and 233 without considering any impacts of traffic getting into the local businesses that pay a great deal of taxes. 11. The chart showing traffic problems at the intersection of Wye and 233 was altered...It was not based upon a ratio of traffic flow to accidents but rather on some other ratio that "better represented" the traffic. I do not like information that has been changed in regards to what some other individual considers a proper format. I request a table showing the number of traffic accidents at each major intersection in Sherwood Park along with the traffic flow. Please provide me the answers that I have requested in bold above.

- iv) We learned from the meeting that this study which brought about the above plan was to consider only transportation requirements based on peak traffic flows. This study and plan are flawed because they did not consider the impact on existing business and flow of traffic outside of peak flow. The overall suggestion of creating three lanes each way on Wye Rd to Hwy 21 is a good idea to help peak flow with traffic returning each week day at 4:30 pm. The west flowing three lanes will do little to help the 7:30 am week day flow into Edmonton because the Sherwood Park Freeway is only 2 lane and the traffic backs up before the clover leaf at Hwy 216. It may even exacerbate the problem. The plan to delete the Wye Road left turn south onto Range Road 233 is terrible. The deletion of Sherwood Drive left turn east bound onto Wye Road and deletion of a left turn from Range Road 233 west bound onto Wye Road is creating more problems unnecessarily. Closing the crossing at WalMart and Ordze and creating a crossing to Rexall off of "jughandle" south of Wye Road at Ash St when it is nearly the same distance from Wye Road is clearly favoritism to this new development and punitive to the existing business on Ordze Ave. Making Wallace Drive an entrance into Ordze Ave is a clear "Slap in the Face" to the existing business and the development procedures agreements that have been negotiated and signed when the business started here. All the buildings face Ordze Ave. The Landscaping Signs, Entrances, Traffic Flow, Drive thru's, Parking, Shipping & Receiving, Snow Storage were all designed to attract and service the customers in Sherwood Park using the traffic design that was created and approved by Strathcona County. These changes will clearly cost the existing Ordze Ave businesses money, growth and property value in favour of a new development elsewhere and a flawed study. In particular, Expert Lock Services will suffer a loss of our Drive thru and a substantial portion of business we created thru it. We will lose our main entrance off Ordze Ave. We will lose our large vehicle and trailer customers with the loss of our west side. We will lose parking for customers and parking for our staff. We will lose snow removal storage. We will lose shipping and receiving parking for large trucks. We will lose our signage facing all our customers. We will have an increase in vandalism and theft from our trucks. We have a lease with the Danley Ventures Ltd. the owners of the building. This building was custom designed and built for us and Brentwood Animal Hospital. Will the County compensate us for our losses over the 25 year renewal lease we have in place? This plan is flawed for these reasons and more I have not listed. Reject these contentious ideas and create a





plan including the existing business. We are the business already here working within the plans and guideline given to us before we opened.

- v) We have expressed our opinions on how we think the plan is flawed and should be reconsidered in other letters from Expert Lock Services Ltd. and Brentwood Animal Hospital. Danley Ventures Inc. owns the property that is leased to these two businesses. This building was custom designed for the two tenants. Every inch of the designing was micro-managed to create exactly what these businesses required for their prosperity, success for the next 25 years and beyond. The lease they hold on this property is clear. Danely Ventures Inc. owns this property to make a profit on their investment. If the Wye Road Plan to annex our property to build a road connection Wallace Drive and Ordze Avenue proceeds we would expect the County of Strathcona to purchase our property outright and compensate us for our loss of income from our leases for the next 25 years and beyond. We will not hold this property in an amputated state. In addition the County as the new owners of this property would be required to compensate the two lessors for their losses in lease rights also over the next 25 years. Please reject this flawed plan so we can concentrate on our business.

## 2.6 VALUE OF MEETING

Stakeholders were asked to provide feedback on the helpfulness of the meeting.

- Among the five individuals who responded to the statement about whether the information in the presentation gave them a good understanding of the proposed Wye Road Plan, three agreed and two disagreed.
- Among the five individuals who responded to the statement about whether the information on the displays gave them a good understanding of the proposed Wye Road Plan, four agreed and one disagreed.
- Among the five individuals who responded to the statement about whether their questions were answered, three agreed and two disagreed.

<b>The information provided in the presentation gave me a good understanding of the proposed Wye Road Plan.</b>		
<b>Answer Options</b>	<b>Response Percent</b>	<b>Response Count</b>
Strongly Agree	20%	1
Somewhat Agree	40%	2
Somewhat Disagree	40%	2
Strongly Disagree	0%	0
<i>answered question</i>		<b>5</b>
<i>skipped question</i>		<b>1</b>

**The information on the displays gave me a good understanding of the proposed Wye Road Plan.**

<b>Answer Options</b>	<b>Response Percent</b>	<b>Response Count</b>
Strongly Agree	20%	1
Somewhat Agree	60%	3
Somewhat Disagree	20%	1
Strongly Disagree	0%	0
<i>answered question</i>		<b>5</b>
<i>skipped question</i>		<b>1</b>

**My questions were answered.**

<b>Answer Options</b>	<b>Response Percent</b>	<b>Response Count</b>
Strongly Agree	0%	0
Somewhat Agree	60%	3
Somewhat Disagree	0%	0
Strongly Disagree	40%	2
<i>answered question</i>		<b>5</b>
<i>skipped question</i>		<b>1</b>



### 3. SUMMARY

Among the 18 stakeholders who indicated their attendance by signing in at the March 19, 2008 Stakeholder Information Meeting, six individuals completed a Comment Form to record their input. That input forms the basis of this document.

The key concerns appear to be as follows.

- The need for improvements to Wye Road is questioned.
- The proposed plan is viewed as devastating to existing businesses, while being potentially more favourable to new businesses, particularly south of Wye Road and west of Sherwood Drive.
- The closure of left-turns westbound to southbound on Sherwood Drive is a major concern, with a lesser concern being the left-turn off Wye Road southbound and eastbound into WalMart.
- The Terms of Reference for the project did not include an assessment of impact on existing businesses, a fundamental gap in the planning of the project.
- It appears that businesses located north of Wye Road but east of Sherwood Drive were not present at the meeting.
- Significant questions remain about how the jughandle will work with respect to Green Street, including e.g. queue length, and at the west leg of Wallace Drive (out of Expert Locks) where a right-of-way is required.

Note that the one County Councilor in attendance agreed to meet with businesses and possibly with the Chamber of Commerce.

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**APPENDIX A  
COMMENT FORM**





***Please tell us whether you agree with the following, and provide any additional ideas you might have about the Wye Road Plan. Thank you!***

**THE WYE ROAD PLAN**

**Strongly Agree      Somewhat Agree      Somewhat Disagree      Strongly Disagree**

- Overall, the **proposed Wye Road Plan** seems like a good concept for Sherwood Park.

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What ideas presented today about the Wye Road Plan do you think will work well, and why?

What ideas presented today do you think Strathcona County should be cautious about incorporating into the Wye Road Plan, and why?

**CLOVERBAR ROAD / NOTTINGHAM AREA**

**Strongly Agree      Somewhat Agree      Somewhat Disagree      Strongly Disagree**

- Overall, the suggestions for the **Cloverbar Road / Nottingham area** seem like good concepts.

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Please provide comments to explain your viewpoint.

**BRENTWOOD BOULEVARD INTERSECTION**

**Strongly Agree      Somewhat Agree      Somewhat Disagree      Strongly Disagree**

- Overall, the suggestions for the **Brentwood Boulevard intersection** seem like good concepts.

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Please provide comments to explain your viewpoint.



**SHERWOOD DRIVE / WALLACE DRIVE /  
ORDZE AVENUE INTERSECTION**

**Strongly Agree      Somewhat Agree      Somewhat Disagree      Strongly Disagree**

- Overall, the suggestions for the **Sherwood Drive / Wallace Drive / Ordze Avenue intersection** seem like good concepts.

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Please provide comments to explain your viewpoint.

**OPEN HOUSE INFORMATION**

**Strongly Agree      Somewhat Agree      Somewhat Disagree      Strongly Disagree**

- The information provided in the presentation gave me a good understanding of the proposed Wye Road Plan.

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- The information on the displays gave me a good understanding of the proposed Wye Road Plan.

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- My questions were answered.

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**ADDITIONAL COMMENTS**

Please provide any additional comments you may have about the Wye Road Plan.

**Please leave this completed form with us today or  
fax it to IMI *strategics* at 780-425-1423.**

**THANK YOU!**





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**APPENDIX B  
MINUTES**





## MINUTES OF MEETING

Job No.: 1149-02  
1154-02

Client : Strathcona County  
Project : Wye Road Widening - Wallace Drive  
Date : March 19, 2008  
Time : 5:30 p.m.  
Place : 2001 Sherwood Drive – Strathcona County Conference Room #1

Purpose : Presentation of Findings of Wye Road Function Planning to Stakeholders

1.0	<b>General</b>
	<ul style="list-style-type: none"> <li>Al-Terra presented the overall findings of the Wye Road Functional Plan which indicates the need for additional lanes on Wye Road and the relocation of some left turn movements at the major intersections (currently proposed via "jughandle" intersection configurations) to improve overall traffic flow and reduce delays on the road.</li> <li>Attendees were then invited to participate in a question and answer session.</li> </ul>
2.0	<b>Questions</b>
	<p><u>Participant #1</u></p> <ul style="list-style-type: none"> <li><i>What percentage of the cost of improvements to Wye Road is borne by the taxpayer and how much is covered by the South of Wye Development/ Developer?</i> The South of Wye Development / Developer and all future Developers along Wye Road are required to pay an arterial road levy. The arterial road levy is based upon the percentage of traffic expected to be generated by the development in comparison to the total estimated traffic along the roadway. The Development is then required to pay a proportion of the road upgrades as a result of the increased traffic. In addition the Developer is required to fully pay for all acceleration / deceleration lanes and/or auxiliary lanes required for the development.</li> <li><i>Did the Wye Road Functional Planning Study include a business impact analysis?</i> The study did not specifically include any business impact analysis or studies. Al-Terra was aware of the impacts on access.</li> <li><i>How many phone calls does the County get per day / week regarding congestion at Sherwood Drive and Wye Road? If Baseline and Broadmoor Blvd. congestion causes many phone calls, how does Sherwood / Wye rate in comparison?</i> Exact numbers are not known at the time of this meeting. The parties present are not in direct receipt of the phone calls.</li> <li><i>Traffic modeling is not an accurate representation of actual traffic and can be tweaked to pre-determine results. The model may not actually truly represent future traffic. Why then must the left be closed when there is not currently a problem at the intersection?</i> Models of existing traffic often depict existing traffic conditions very accurately. The left turn closures are proposed to be staged in 2010 or later dependant on a performance monitoring.</li> <li><i>How does the Wye / Sherwood Drive intersection rate in terms of collisions?</i> An exhibit showing collision statistics for Baseline Road and Wye Road is displayed. The intersection has the 4<sup>th</sup> highest crash rate of the seven major intersections on Baseline Road and Sherwood Drive.</li> <li><i>Is someone taking minutes? Will they be available to the Participants and/or Council?</i></li> </ul>

Yes.

- *As the Wal-Mart Development Agreement approved the left turn into the Wal-Mart site from Sherwood Drive (SB to EB), would the closure have legal ramifications, as it would be contravening the legal obligations outlined in the Development Agreement? The Participant intends to consult his legal counsel.*

It has been the understanding of the respondents present that an access must be maintained, but the form, all-directional, right-in / right-out, is discretionary.

#### Participant #2

- *If the left turns impact the through traffic in the PM Peak, then perhaps the left turns should be prohibited during the peak hours only.*
- *Participant reiterated that he drives through the intersection (Sherwood Drive / Wye Road) and turns left (WB to SB) every morning at 7a.m. and never experiences any delays.*
- *Participant expressed concerns that he is "wasting his time" and that the decision has already been made.*

Respondents indicated that these roadway improvements form the recommendation that the Engineering Department will take forward to Council as the solution to congestion along Wye Road. The decision is ultimately one of Council and they will be fully informed of the business owners concerns. Business owners are also encouraged to speak to their Councilors on the matter. All comment cards will be passed on to Council.

- *Participant indicated that although he concurs the proposal will address congestion along Wye Road it will be at the expense of the business owners and he anticipates it will result in business failure / bankruptcy.*

#### Participant #3

- *Area structure plan displayed at the South of Wye presentation to Council did not show the deletion of the left at Sherwood Drive and Wye Road, nor did it show the jughandle connection around the south side of Wal-Mart.*

Respondent cannot comment on the material presented at that meeting. It was not prepared by Al-Terra Engineering and they were not in attendance. Council was presented the proposed Wye Road improvements and Wallace Drive jughandle concept in October 2006.

- *Participant requested clarification that the ultimate decision to proceed on the project would be made by Council.*

#### Participant #4

- *Participant cited that in his experience in California, there are multiple lane intersections with multiple left turn lanes and no "jughandle" intersections, and traffic flows fine.*
- *Participant cited that he has "never missed dinner" because of the congestion at any of the Sherwood Park intersections.*

Respondent indicated that the residents and commuters of Sherwood Park have very high expectations regarding traffic flow. While residents in California may be content to sit through several cycles of lights, the traveling public in Sherwood Park has higher expectations. There are numerous intersections within the City of Edmonton that are operating at a Level of Service "F" and there are no immediate plans to improve the intersections. The traveling public must live with the situation or find an alternative route. Strathcona County is attempting to be proactive and avoid such congestion.

#### Participant #5

- *Does not agree with the movement of the left turn (WB to SB) at the RR 232 / Wye Road intersection, as in his experience the movement is not heavy and does not experience significant peaks.*



Participant #6

- *The Green Street intersection on Wallace Drive jughandle is at approximately the same spacing at the exiting Wal-Mart intersection. Participant questioned as to why that intersection was anticipated to work and was approved when the existing Wal-Mart intersection does not function well and is proposed to be closed.*

Respondent indicated that it would be preferable not to have the Green Street intersection along the Wallace Drive jughandle. However, the Developer traffic modeling has indicated that the queues will not impact Wye Road, and that the intersection will not have to be signalized. Council has approved the Green Street intersection.

- *Did the study consider and/or include traffic impact that an "outer ring road" and/or the twinning of Highway 628 would have?*

The traffic modeling for the 2015 horizon considered the impacts of the adjacent roadways and alternative travel routes.

- *What are the design plans for the berm on the north side of Wallace Drive west of Range Road 233? Are there plans to remove and/or lower the berm so that business owners can have visual access to the roadway? How will noise attenuation be addressed for the landowners on the south side of the road?*

Detailed design has not been completed to date on the section of Wallace Drive west of Range Road 233. Preliminary design indicates that the berm and fence may be removed, and relocated to the south side of the roadway.

- *How will the grade difference between the back of the businesses and the Wallace Drive roadway be addressed?*

The cross-section will be urban and the drainage will be maintained within the road right-of-way and not impact the business properties. The existing grade of Wallace Drive will likely be lowered.

- *Participant expressed concern as to the loss of a portion of his property for the road construction. Participant was adamant the loss of the property, and specifically the drive through key cutting window, would eminently cause his business to fail.*

Participant #7

- *Participant frequently drives NB on Sherwood Drive and completed left turns onto Fir Street. In his experience there is an accident at that intersection every week. What are the plans for that intersection and how does the proposed design intent to mitigate the accidents?*

Proposed improvements at the intersection of Fir Street and Sherwood Drive include the introduction of a longer NB to WB left turn lane in conjunction with the closure of the access into the "Old Shopping Center" south to the Fir Street intersection, as well as a double left SB to EB.

Participant #8

- *Appreciative of the Participants concerns regarding impact to business. Will ensure that the concerns presented in this meeting are passed on to Council.*
- *Would support a study on how the road improvements would potentially impact businesses.*
- *Suggested a possible presentation to the Chamber of Commerce.*

**Meeting Concluded.**

Please report errors or omissions to the undersigned.

Minutes Prepared by:  
**Al-Terra Engineering Ltd.**  
Dana Leithead, P.Eng.

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**APPENDIX C**  
**NOTIFICATION LIST**



**HAND-DELIVERED (possibly incomplete or missing addresses)**

Carpet Showplace
Crimpz Bead Boutique
Dr. Bernstein Diet & Health Clinic
H&R Block
ICI Paints
Liquor Depot
Method Acoustics
Park Cycle
PetroCanada
Pizza Hut
Rental Dan
TD Canada Trust
Team Realty Execs
Telus Mobility
Trail Blazers
Village Park Medical Clinic
Winning Wines Plus

**LANDOWNER MAILINGS (including some ALSO hand-delivered)**

HAND-DELIVERED	LANDOWNER NAME	ADDRESS1	ADDRESS2	ADDRESS3	CITY	CODE
	1013224 ALBERTA LTD	13138 50 ST			EDMONTON	T5A 4Z8
	1053163 ALBERTA LTD	983 FIR ST			SHERWOOD PARK	T8A 4N5
	1159937 ALBERTA LTD	SHERWOOD PARK PROFESSIONAL	912 ASH ST		SHERWOOD PARK	T8A 2G1
	1215796 ALBERTA LTD	307 CORAL COVE NE			CALGARY	T3J 3Y9
	1252902 ALBERTA LTD	197 ORDZE AV			SHERWOOD PARK	T8A 2T4
	423564 ALBERTA INC	C/O 76 WILLOW PARK GREEN SE			CALGARY	T2J 3L1
	629055 ALBERTA LTD	105 ORDZE AVENUE			SHERWOOD PARK	T8A 1M1
	654210 ALBERTA LTD	194 ORDZE AVENUE			SHERWOOD PARK	T8B 1M6
	654210 ALBERTA LTD	194 ORDZE AVENUE			SHERWOOD PARK	T8B 1M6
	685999 ALBERTA LTD	ATTN: NICK TKACHENKO	110 ORDZE AVE		SHERWOOD PARK	T8B 1M6
	741662 ALBERTA LTD	97-51313 RGE RD 231			SHERWOOD PARK	T8B 1K7
	767228 ALBERTA LTD	100 - 12222 - 137 AVE			SHERWOOD PARK	T5L 4X5
	799766 ALBERTA LTD	918 ASH ST			EDMONTON	T8A 2G1
	799766 ALBERTA LTD	918 ASH ST			SHERWOOD PARK	T8A 2G1
	815720 ALBERTA LTD	C/O JIM KULAK	106 3802 49		STONY PLAIN	T7Z 2J7



HAND- DELIVERED	LANDOWNER NAME	ADDRESS1	ADDRESS2	ADDRESS3	CITY	CODE
	899992 ALBERTA LTD	18 FLR 1067 WEST CORDOVA ST	AVENUE		VANCOUVER	V6C 1C7
	948619 ALBERTA INCORPORATED	930 ASH STREET			SHERWOOD PARK	T8A 2G1
	BURNLEY BUSINESS ENTERPRISES LIMITED	192 ORDZE AVE			SHERWOOD PARK	T8B 1M6
	CANADA TRUSTCO MORTGAGE COMPANY	12 FLR 380 WELLINGTON ST			LONDON	N6A 4S4
HD	CANADIAN TIRE CORPORATION LIMITED	ATTN: JIM WILKS	2180 YONGE ST	PO BOX 770 STN K	TORONTO	M4P 2V8
	DANLEY VENTURES INC	183 ORDZE AVE			SHERWOOD PARK	T8B 1M6
	FIRST CAPITAL (CANHOLDINGS) CORPORATION	C/O FCB PROPERTY MANAGEMENT SV	7400 BIRCHMOUNT RD	ATTN: JOANIE MORRA	MARKHAM	L3R 4E6
	FIRST CAPITAL (CANHOLDINGS) CORPORATION	C/O FCB PROPERTY MANAGEMENT SV	7400 BIRCHMOUNT RD	ATTN: JOANIE MORRA	MARKHAM	L3R 4E6
	IMPERIAL OIL LIMITED	P O BOX 422	DON MILLS STATION		NORTH YORK	M3C 2T2
	JIM PATTISON DEVELOPMENTS LTD	1800-1067 WEST CORDOVA ST			VANCOUVER	V6C 1C7
HD	MC DONALD'S RESTAURANTS OF CANADA LTD	2 MCDONALD'S PLACE	ATTN: REAL ESTATE SS 104/60	MSIS 2061	TORONTO	M3C 3L4
HD	MEGA CAR WASH LTD	129 ORDZE AVE			SHERWOOD PARK	T8A 6C4
	PARK AVENUE SPECIALTIES LTD	191 ORDZE AVE			SHERWOOD PK	T8B 1M6
	SHELL CANADA LIMITED	ATT: ROB MATIKO	PROPERTY TAX ANALYST	400-4TH AVE S.W.	CALGARY	T2P 2H5
	SHERWOOD PARK AUTO CENTRE	BOX 41011	PETROLIA RPO		EDMONTON	T6J 6M7
	SHERWOOD PLAZA INC	C/O FCB PROPERTY MANAGEMENT SV	ATTN: JOANIE MORRA	7400 BIRCHMOUNT RD	MARKHAM	L3R 4E6
	STRATHCONA COUNTY	2001 SHERWOOD DR			SHERWOOD PARK	T8A 3W7
	TONECRAFT CORPORATION	156 LAKESHORE RD E SUITE 200			OAKVILLE	L6J 1H4
	VILLAGE PARK HOLDINGS INC	55 VALLEYVIEW CRES	BOX 100 SITE 212 RR2		ST ALBERT	T8N 1M9
	WAL-MART CANADA INC	1940 ARGENTIA RD			MISSISSAUGA	L5N 1P9

## **BUSINESS MAILINGS**

<b>HAND- DELIVERED</b>	<b>BUSINESS NAME</b>	<b>UNIT/BUILDING</b>	<b>STREET NUMBER</b>	<b>STREET NAME</b>	<b>STREET TYPE</b>	<b>SUFFIX</b>	<b>CODE</b>
HD	A Ride in the Park Saddle		196	ORDZE	AV		
	A&D Daycare	101	937	FIR	ST		T8A-4N6
HD	A&W		99	WYE	RD		T8A-1M1
	Aerotek Computers		918	ASH	ST		T8A-2G1
HD	Alberta Treasury Branch		201	WYE	RD		T8B-1N1
HD	All Tax & Accounting Inc.	108	957	FIR	ST		T8A-4N6
	Bank of Montreal		145	ORDZE	AV		
	Beijing City Chinese Restaurant	138	1020	SHERWOOD	DR		T8A-2G4
	Bernies Cellular Wireless Communication Sales, Rent, Service		973	ORDZE	RD	X	T8A-4L7
	BJ Brew Crew	302	975	FIR	ST		-
	Bob The Jeweler		930	ASH	ST		T8A-2G1
	Boston Pizza Ltd.		967	ORDZE	RD		T8A-4L7
HD	Bowling Fun Centre & Restaurant	301	975	FIR	ST		-
	Boyd's Ocean Fresh Sea Food Restaurant	103	977	FIR	ST		-
	Boys & Girls Club	Sherwood Park Dental Care Centre	922	ASH	ST		T8A-2G1
HD	Brentwood Animal Hospital		185	ORDZE	AV		
	Brentwood Chiropractic Clinic	108	937	FIR	ST		T8A-4N6
	Caffrey's Irish House	1	99	WYE	RD		T8A-1M1
HD	Cali Nails	84	993	FIR	ST		T8A-4N5
	Canada Trust		139	ORDZE	AV		
HD	Canadian Driveline Inc.		110	ORDZE	AV		
	Canadian Tire Gas Bar		167	ORDZE	AV		
HD	Canadian Tire Store		169	ORDZE	AV		T8B-1M6
HD	Carpet Showplace		120	WYE	RD		-
HD	Chamber of Commerce		100	ORDZE	AV		T8B-1M6
HD	CIBC	250	1020	SHERWOOD	DR		T8A-2G4
	Cleaning by Page/Canada Post	Sherwood Park Professional Building	926	ASH	ST		T8A-2G1
HD	Climb Higher Performance	206	979	FIR	ST		-
	Colour Your World		193	ORDZE	AV		
	Cosmos Greek Taverna	180	1020	SHERWOOD	DR		T8A-2G4
HD	Crazy 8's	15	1020	SHERWOOD	DR		T8A-2G4



HAND- DELIVERED	BUSINESS NAME	UNIT/BUILDING	STREET NUMBER	STREET NAME	STREET TYPE	SUFFIX	CODE
	Crimpz Bead Boutiques		99	WYE	RD		T8A-1M1
HD	Cruise Ship Centers		967	ORDZE	RD	A	T8A-4L7
HD	Dairy Queen		192	ORDZE	AV		T8B-1M6
HD	Dispensaries (1991) Ltd.		937	FIR	ST		T8A-4N6
HD	Domo Gas Bar	45	99	WYE	RD		T8A-1M1
HD	Douglas Environmental Solutions	109	957	FIR	ST		T8A-4N6
	Dr. Bernstein (Diet & Health Clinics)						
	Dr. Joyce Romanowski	104	937	FIR	ST		T8A-4N6
	Dr. Phillip Sych	104	937	FIR	ST		T8A-4N6
HD	Earls Restaurant		194	ORDZE	AV		
	Earthmaster Environmental Strategies Inc.		937	FIR	ST		T8A-4N6
	Eastgate Physical Therapy Clinic	100	937	FIR	ST		T8A-4N6
HD	Everything For A Dollar Store	21	993	FIR	ST		T8A-4N5
HD	Expert Lock Services Ltd.		183	ORDZE	AV		
HD	Extreme Pita	24	993	FIR	ST		T8A-4N5
HD	Eye-Bar	25	993	FIR	ST		T8A-4N5
HD	Fabutan	89	993	FIR	ST		T8A-4N5
	Fas Gas	100	977	FIR	ST		-
HD	Fir Street Bottle Depot	201	979	FIR	ST		-
	For Your Eyes Only	102	937	FIR	ST	B	T8A-4N6
HD	H & R Block		1020	SHERWOOD	DR		T8A-2G4
	Harris Eye Clinic		937	FIR	ST		T8A-4N6
HD	HealthWorks Nutrition Centre	200	1020	SHERWOOD	DR		T8A-2G4
	Herbal Magic	11	99	WYE	RD		T8A-1M1
HD	Home Building Center		197	ORDZE	AV		
HD	In Hair Salon	17	993	FIR	ST		T8A-4N5
HD	In More Detail	109	977	FIR	ST		-
	InstaLoan	13	99	WYE	RD		T8A-1M1
	Insurance Discounts Ltd.	204	937	FIR	ST		T8A-4N6
	Iris Evans MLA	116	937	FIR	ST	B	T8A-4N6
	Jumbo Car Wash		129	ORDZE	AV		
	Kentucky Fried Chicken (The Bucket)	30	1020	SHERWOOD	DR		T8A-2G4
	Kodiak Engineering Ltd.	101	957	FIR	ST	A	T8A-4N6
HD	Lakeside Vet Clinic	93	993	FIR	ST		T8A-4N5
	Liquor Inn	17	99	WYE	RD		T8A-1M1
HD	Liquor Depot						
HD	London Drugs	999	993	FIR	ST		T8A-4N5
	Lorendys Organic Market		916	ASH	ST		T8A-2G1
	Love Boutique	B	971	ORDZE	RD		T8A-4L7

HAND-DELIVERED	BUSINESS NAME	UNIT/BUILDING	STREET NUMBER	STREET NAME	STREET TYPE	SUFFIX	CODE
HD	Lube X	30	993	FIR	ST		T8A-4N5
	M&M Meat Shops Ltd	73	993	FIR	ST		T8A-4N5
HD	Mattress Mattress	157	1020	SHERWOOD	DR		T8A-2G4
HD	McDonalds Restaurant		950	ORDZE	RD		T8A-4L7
	McGavins Bread Basket	19	99	WYE	RD		T8A-1M1
	McMan Youth Services		971	ORDZE	RD		T8A-4L7
HD	Midas	121	120	WYE	RD		-
	Miyako Japanese Restaurant	80	1020	SHERWOOD	DR		T8A-2G4
	Museum Strathcona County & Arch		913	ASH	ST		T8A-2G3
HD	NAPA Auto Parts	20	1020	SHERWOOD	DR		T8A-2G4
	Palomino Saloon	116	957	FIR	ST		T8A-4N6
	Panagopoulos	23	99	WYE	RD		T8A-1M1
HD	Paramount Dry Cleaners	85	993	FIR	ST	C	T8A-4N5
	Park Avenue Specialties		191	ORDZE	AV		
HD	Park Drycleaners		963	ORDZE	RD		T8A-4L7
HD	Park Mazda		983	FIR	ST		-
HD	Payless ShoeSource		203	WYE	RD		T8B-1N1
HD	Pennington's Superstore		993	FIR	ST		T8A-4N5
HD	Picture This Framing Gallery		959	ORDZE	RD		T8A-4L7
HD	Pizza Hut	60	993	FIR	ST		T8A-4N5
	Regal Hair Design	A	969	ORDZE	RD		T8A-4L7
HD	Registry On Wye	25	99	WYE	RD		T8A-1M1
HD	Reitmans	80	993	FIR	ST		T8A-4N5
	Richards Donair	31	99	WYE	RD		T8A-1M1
HD	Rogers Video	5	1020	SHERWOOD	DR		T8A-2G4
HD	Rogers Wireless	18	993	FIR	ST		T8A-4N5
	Royal Lepage		955	ORDZE	RD		T8A-4L7
HD	Safeway		985	FIR	ST	X	-
	Safeway Gas Bar		985	FIR	ST		-
HD	Save On Foods (Fir)		81	FIR	ST		-
HD	Scotia Bank	15	993	FIR	ST		T8A-4N5
HD	Second Cup	113	120	WYE	RD		-
HD	Sherwood Exhaust	41	99	WYE	RD		T8A-1M1
HD	Sherwood Park Bakery (1992) Ltd.		969	ORDZE	RD		T8A-4L7
	Sherwood Park Professional Building		912	ASH	ST		T8A-2G1
	Sherwood Park Shell		23263	WYE	RD		-
HD	Sherwood Print & Copy	33	99	WYE	RD		T8A-1M1
HD	Shig's Flowers		945	ORDZE	RD		T8A-4L7
	Silk Seam Tailor	114	937	FIR	ST		T8A-4N6

HAND- DELIVERED	BUSINESS NAME	UNIT/BUILDING	STREET NUMBER	STREET NAME	STREET TYPE	SUFFIX	CODE
HD	Smilies Restaurant		981	FIR	ST		-
HD	Sorrentinos	160	1020	SHERWOOD	DR		T8A-2G4
HD	Spark Heating & Air Conditioning Ltd		115	ORDZE	AV		T8B-1M6
	Strathcona County Transit		970	ORDZE	RD		T8A-4L7
	Subway	101	120	WYE	RD		-
HD	Sunclip Express		947	ORDZE	RD		T8A-4L7
	Team J Realty Exexcutes Progressive	206	937	FIR	ST		T8A-4N6
	The Liquor Depot	75	993	FIR	ST		T8A-4N5
HD	The Water Warehouse	109	120	WYE	RD		-
HD	Tim Hortons	90	993	FIR	ST		T8A-4N5
HD	TireCraft		105	ORDZE	AV		T8A-1M1
	Todays Image	107A	957	FIR	ST		T8A-4N6
HD	Trail Blazers		120	WYE	RD		-
HD	Vickys Family Dining	86	993	FIR	ST		T8A-4N5
	Village Park Mall		937	FIR	ST		T8A-4N6
	Village Park Medical Clinic	110	937	FIR	ST		T8A-4N6
HD	Vince's Auto Care	210	979	FIR	ST		-
HD	Walmart		239	WYE	RD		-
HD	Wendy's Restaurant	201	198	ORDZE	AV		T8A-3H9
HD	Western Boots Shoes & Leather Repair	35	1020	SHERWOOD	DR		T8A-2G4
HD	Wild Rose Florist	50	1020	SHERWOOD	DR		T8A-2G4
	Wood Doctor, The	107	977	FIR	ST	X	T8A-4N5
HD	WSG Insurance Services	27	99	WYE	RD		T8A-1M1

## Appendix D.2

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Proposed Wye Road Plan Functional Planning Study,  
May 2008 Public Open House Summary  
*(IMI Strategies) – 2008*





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**PROPOSED WYE ROAD PLAN  
~ FUNCTIONAL PLANNING STUDY ~**

**MAY 2008 PUBLIC OPEN HOUSE  
SUMMARY**

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*prepared for*

**Strathcona County**

May 30, 2008



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## 1. INTRODUCTION

An Open House for the public was held at the Strathcona County Offices on May 7, 2008 for the purpose of presenting the proposed Wye Road Plan (the Functional Planning Study) to community members. A total of 525 invitations were mailed to community members directly affected by the proposal. An additional 20 invitations were hand-delivered due to incomplete mailing addresses. A total of 134 people attended the Open House.

This document provides results obtained from Comment Forms made available to Open House attendees. The Comment Form asked individuals to indicate their level of agreement or satisfaction with aspects of the proposed plan, as well as to provide written comments. Since Open House attendance was by invitation, the results derived from the Comment Forms cannot be interpreted as statistically representative of Wye Road users or of Strathcona County residents in general.

Sixty-eight completed Comment Forms were received. Section 2 of this document provides input from these forms with regard to specific road and intersection proposals, presented first in chart form for each aspect of the proposed plan addressed on the Comment Form. The charts are followed by verbatim written comments associated with the numeric responses.

Section 3 provides chart results pertaining to proposed modifications for pedestrians and cyclists. Section 4 describes how attendees currently use Wye Road. Section 5 provides feedback on the value of the Open House.

Appendix A contains the Open House Comment Form prepared by IMI *strategics*.

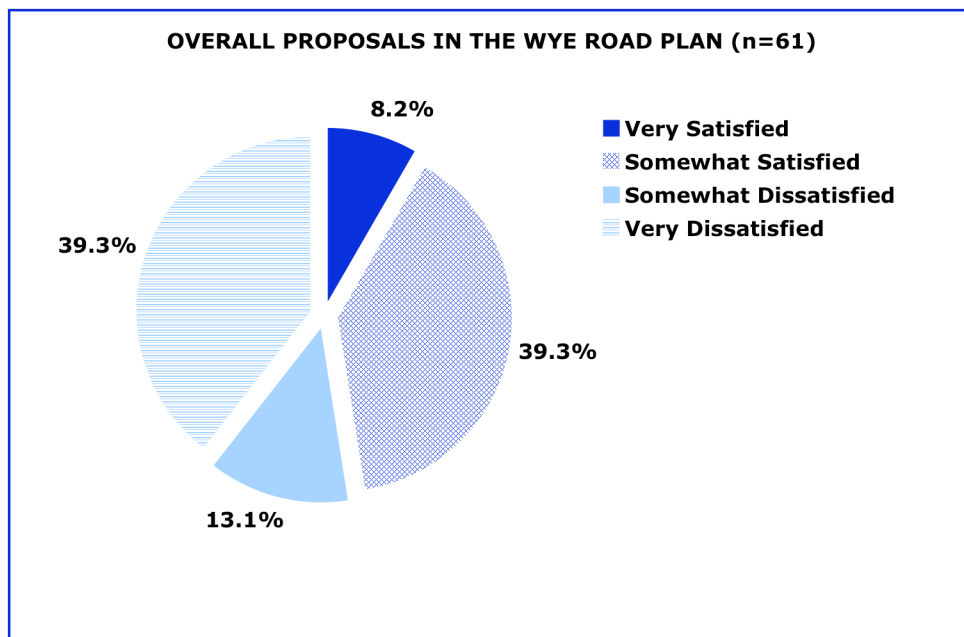


## 2. PROPOSED WYE ROAD PLAN

A series of statements were provided on the Comment Form about aspects of the Wye Road Plan, to which stakeholders could provide feedback by indicating their level of agreement or satisfaction, and provide comments. The responses received using the Comment Form are set out here first in chart form, and are followed by written comments associated with the statement.

### 2.1 OVERALL PROPOSALS IN THE WYE ROAD PLAN

Among the 61 individuals who responded to the statement about whether they were satisfied with the overall proposals in the Wye Road Plan, 48% were satisfied and 52% were dissatisfied.





*Very Satisfied*

- Great deal. Safety and traffic concerns to RR 233 rural residents! Thanks.
- The County should have a great transit system to all areas of Edmonton to eliminate traffic.

*Somewhat Satisfied*

- Anything that we can do to make Wye Road better/visually is a good thing.
- I have a concern with the jug handle on RR 232 with the new residential development coming, we will have 4 intersections between Estate Drive and Wye Road. Why not combine one or two of these? The jug handle intersection is too close to Wye Road and I can see traffic backing up on 232 for enough to block the jug handle intersection.
- Is going to divert more traffic down 233. The noise on 233 is terrible even with slower limit.
- It will be a better flow of traffic at the "bloody" Walmart corner.
- Lack of people to explain the overall modifications. Handout lacked detail.
- More consideration for sound barrier besides Estates Village on Wye Road and RR 232. County must accept responsibility for this - not developer who was told Wye Road would not be widened.
- Proposal is valid if there will be substantial growth to the area as long as trails, trees and proper buffer walls (cement) are added.
- Some concern about traffic noise. Big tanker trucks going down Wye Road to Hwy 21.
- Some protection from traffic noise is needed.
- The sole focus of this appears to be on the moving of east-west traffic flow. I agree this is important but I fear catering to the driver will rule the day, i.e. no emphasis on reducing speed, ambiance will be forfeited and pedestrian and bike traffic will be overlooked.
- West of Ordze (McDonald's) still appears to choke down to two westbound lanes, so no matter the efficiency of the Wye Road plan, heading into Edmonton in any traffic would be better improved first.

*Somewhat Dissatisfied*

- Al-Terra seems to have made the best accommodation they can to a flawed County policy. When are we going to get tough with the big-box mall developers and keep our limited access major thoroughfares just that?
- Does not appear too much consideration has been given to increased noise factors.
- I don't think jug handles will solve anything apart from causing confusion. It makes the left turns a challenge.
- In this time of discouraging road travel, this development speeds up commuting and road access. This seems a lot of money spent to encourage road use. The jug handles in general will cause drivers to go out of their way to avoid them. This means they will be more apt to use Fir and Alder in going west to east thus moving more traffic to these residential streets.
- Until the issues of the corner of Fir Street and Sherwood Drive are resolved the corner of Wye Road and Sherwood Drive will always be problematic. The problems



that will be created from Fir to access Save-On and other businesses have not been adequately addressed, as Save-On delivery vehicles disallow access from Fir Street on a regular basis. The Ash Street entrance to the Save-On shopping area and CIBC is too awkward to be used regularly by many vehicles and cars turning there will create traffic tie-ups going north from Wallace/Wye Road onto Ash Street. On-street parking on Ash Street and on Fir Street behind Safeway will need to be suspended when they become main thoroughfares. Where will transit passengers be expected to park?

- While trying to make left turns easier at the lights and not past the businesses, it will make lineups even longer on the service road and more accidents. I avoid this area now. I certainly will hesitate to shop there after this is complete.
- Wye Road will have too many traffic lights and will be a real headache just like Baseline has become. You are using too much land for the roads and not leaving enough for proper aesthetic value i.e. trees, bushes, berms and fences. Baseline looks and Wye Road is such a major road and will not be anywhere near as beautiful.

### *Very Dissatisfied*

- A bandaid job that is a poor excuse to a foreseeable problem.
- As per usual, you are blowing taxpayers' money on frivolous, useless modifications. Double lane 233 if you want to do something useful. You have a bunch of "wannabees" in the County with minimal experience in this sort of thing. Just because they have a title with "engineer" in it, it does not mean they are any good. Anyone can see these ideas are nonsense. Block off the Walmart entrance on 233, widen 233 and put the Walmart entrance by the Shell station and use the one on Wye Road as well. All you are doing is pandering to Walmart at the expense of the smaller businesses and the people who pay your salaries. You put us through the charade of an open house etc. when you have made your minds up anyway! The jug handle will just move the congestion down to Ordze Road and on the service road. Our property values will decrease and the very reason we bought here is threatened. Wye Road intersection is fine, just take out the left turn south into Walmart and block it off completely, lights included.
- Could not get answers to questions re noise levels, property values etc. Will be too many lights. Another example of piece-meal planning by the County. Why looking at 10% space capacity, is this enough? Are we going to be doing this again in five years?
- Direct traffic out of the Park (or through) when should keep it in. Traffic bottleneck at Sherwood Park Freeway. Does not consider current businesses on Ordze Avenue - ridiculous access. Redistribute traffic inadequate leading to other problems.
- I don't think there is a need for it as there really is no room for more housing going east or west, so the flow of traffic won't get heavier. Adding more lanes and more lights will cause more traffic volume because of constant lights in shorter spaces.
- I suggest you send a personal letter to all residents between 232 and 233 and Wye Road to TWP 520 to share your plan!
- Impedes easy access to our business (Dairy Queen). Removal of our west entrance/exit. We need two entrance/exits as per our development 18 years ago. We lose access to our drive-through (50% of our business). Lack of consultation over past two years since this "plan" was started. The economic consequences to



our business are enormous. The viability of the business and the other businesses in the immediate area is of critical importance to us and should be to the Council and administration of Strathcona County.

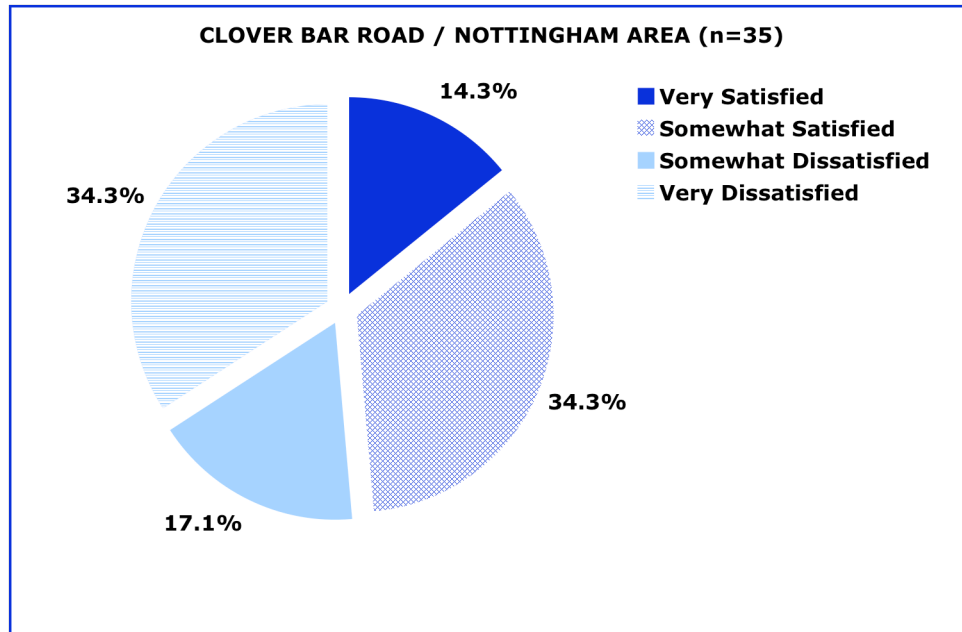
- It is completely ludicrous. Basically you are sacrificing the convenience of the residents and businesses of Sherwood Park in favour of that of the residents of Edmonton. The problem would just be moved down the road anyway! Leave it be!
- Jug handles should be kept on jugs, where they serve a useful purpose. Replicating the problem intersections south of Wye Road at Queen Street will cause further problems requiring a jug handle on a jug handle perhaps!
- New developments require service roads. Access to Wallace Drive restricted to 233 traffic, no local access. Increased traffic is a barrier to all non-motorized traffic. Traffic flow patterns not coordinated with public transit. High density traffic will develop on First and Fir Streets. Will seniors centre be closed? It will become dangerous. Retail access drives are not addressed. Can the County afford to expropriate or subsidize affected residential properties? The project scope is too small. Not all the ramifications are presented well.
- No communication about the loss of access to our driveway and property from Wallace Drive!
- No input into process even though we are directly affected. Was promised a berm and fence on our side of the road. Different people have different answers.
- Noise - aesthetics does not mitigate noise levels.
- Question of whether the "cure" is worse than the problem. Jug handles appear to work well where they are solely concerned with moving traffic around a corner such as on Baseline and 17 Street. However, in the instant cases of the jug handle at Wallace/Walmart and at Brentwood/RR 232 off of Wye and around the DEVCO lands, both have or will have entrances/exits to retail areas, thus impeding the flow of traffic as per the following for Wallace: - traffic across Ash Street from Green Street to and from the Shell Service Station - traffic to and from Walmart off of Wallace - Walmart trucks will exit right from Walmart on to Ash Street, the jug handle or exit Walmart on to RR 233 turning right, then will not be able to turn left on Wye Road, will have to turn left at the London Drugs/Sherwood Dr/Fir Street intersection, presumably retail customers can also exit right from Wal-Mart on to RR 233 and also have to go the noted intersection to turn left to head back to Wye via Fir. This will increase congestion at Sherwood and Fir by London Drugs. Normal traffic coming north on RR 233 will likely also to go Sherwood and Fir and turn left rather than turn left on the Wallace jug handle and go up to Ash Street in order to turn left on Wye Road. The latter route takes you away from your designated direction (west) while going to the Sherwood/Fir corner and then turning left has the feel of being more direct/closer to your designated direction. I understand that right turns will still be allowed out of the old shopping centre (video/PetroCan, CIBC etc.) location. It is difficult now to exit right from that location and make it across Sherwood Drive to get into the left hand turn lane and turn left at the lights at Sherwood/Fir (London Drugs). The foregoing and the traffic from RR 233 and from Walmart heading to that corner to turn left will create major issues. The jug handle at the Brentwood/RR 232 corner appears to semicircle the currently undeveloped DEVCO lands which is zoned for commercial. The only way to access/exit those lands will be off of the jug handle which will slow traffic considerably, again different from the 17th Street/Baseline situation. Impacts on previously established businesses like Home Hardware could be significant re public accessibility perceived and real. If the plan is to proceed,

building stages with formal approvals of other stages subject to future Councils, political consideration et al, is a major concern. As a whole, the plan has potential problems but the piecemeal approach raises the possibility that some other parts of the plan may never be implemented, for example, the road connection between the Canadian Tire/Locksmith operations is really the other half of the Wallace jug handle - the other side of the coin - and if it is never done for whatever reasons, we are all stuck with half of the plan with all of the pressure/traffic on the Wallace jug handle and negative impact on adjacent residential properties. Disappointing that RR 233 changes which have been approved weren't actually shown as to how they coordinate/integrate with the rest of the plan. Some specific questions re the RR 233 highlighted issues re school buses picking up children as well as garbage pick-up (continue to have to stop on RR 233) despite the existence of a service road along Campbelltown Heights. Overall plan is complex with no left or right turns where people would normally expect to have them and vice versa (have to turn before the point you would normally expect - the "locals" will ultimately get used to it but non-locals will always be confused. In the instance of the Wallace jug handle, you have to turn right in order to eventually turn left - this will not seem natural or logical. All of the proceeding can contribute to accidents. Very disappointing that the "open house" was essentially a public relations exercise in respect of the first stage of the plan - the Wallace/Ash street jug handle. Notwithstanding any of the viewpoints relative to these specific parts relative to the overall plan, the aforementioned jug handle has already been approved and will proceed regardless. The irony is that if the rest of the plan fails to proceed for whatever reasons in the future, the Wallace jug handle is left to take the brunt of the whole. Notwithstanding prior approval of the Wallace jug handle, details as to the actual construction and measures to address concerns of the adjacent residents/subdivision were decidedly lacking.

- Totally ridiculous with the way the traffic will be diverted from the subdivisions - e.g. Wallace Drive to Campbelltown Heights - in a complete circle to the east and then go on to west - the short distance between the two intersections??? Foolish!!
- Traffic volume east of 231 does not merit three lanes. A major 3-lane thoroughfare takes away from the community feel.
- Unbelievably unrealistic! Who got a stipend to allow this road plan??
- We moved into a quiet residential area and now you want to put us on main traffic road.
- Why ask for comments if it is going ahead anyway. Why ask us now? Why not before? A waste of money. I don't think there is a need. Sherwood Park has several ways to access Edmonton, unlike St. Albert. Don't ask people to put time and energy into a decision that has already been made. You're not going to change plans anyway! I was told the plans were going ahead, so why insult me by giving the illusion that what I think will make any difference?

## 2.2 CLOVER BAR ROAD / NOTTINGHAM AREA

Among the 35 individuals who responded to the statement about the Clover Bar Road/Nottingham area, 49% were satisfied and 51% were dissatisfied.



### *Very Satisfied*

No comments.

### *Somewhat Satisfied*

- Looks like the added traffic flow will be taken care of with the loop.
- The potential jug handle at the Clover Bar/Wye location may work somewhat better as it does not go around commercial/retail areas with exists/access. Since there is a school, there may be some issues re school buses entering/leaving particularly in the AM during high morning traffic times. The design/plan at the Nottingham entrance was difficult to evaluate, it appears to be connected to future commercial/retail development on the opposite (south) side of Wye.
- Would like to see an itemized list of specifics that are being considered.

*Somewhat Dissatisfied*

- No direct contact with owner to assess impact of future development.
- No realistic date for start or completion.
- The jug handles will add three lights to turn left instead of one.

*Very Dissatisfied*

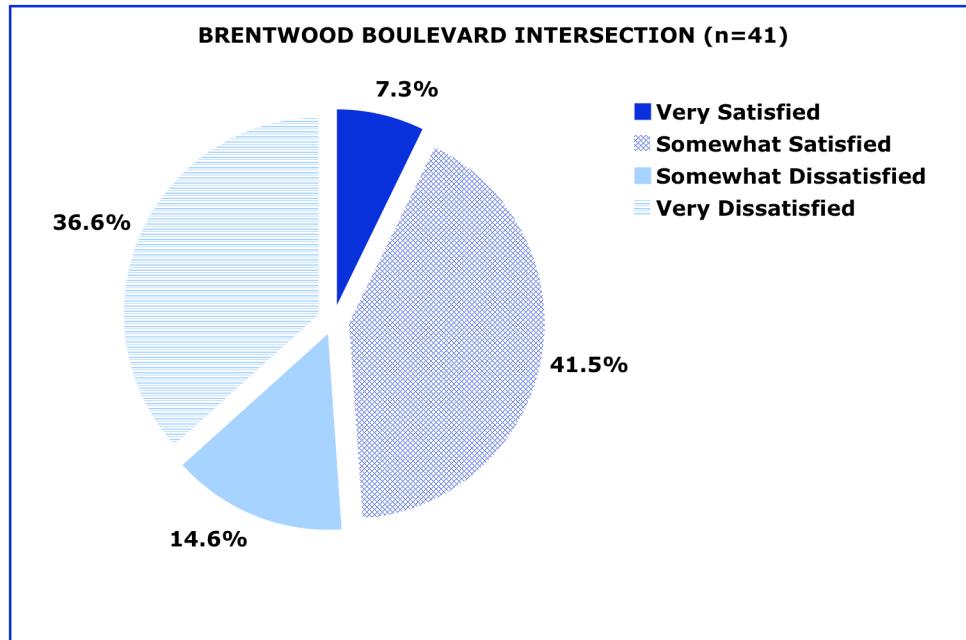
- Given the current and proposed traffic volumes on this stretch of roadway (i.e. east of RR 231), I don't see the need to triple the roadway. It seems like a waste of tax payers' money.
- Going from two lanes to three lanes is not the problem. It's the polluting big three and five ton trucks that will elevate dangerous chemicals as well as noise. Put a ban on vehicles weight and size. Please! Or lower our taxes as we thought we checked with the County about Wye Road improvements before we built. Another broken promise.
- I was too upset about how these changes are going to affect my entrance to 233. I couldn't concentrate on what was proposed for this or Brentwood's intersections.
- No need for three lanes. Development east and this northeast node will not result in excess traffic. If the County is doing this they should provide some noise barrier for residents to the south to compensate for this intrusion.
- Range Road 231 north-south traffic will be left as an afterthought sometime in the future. The speed on 231 needs to be controlled. This Wye Road focus is important but all other connected issues are ignored.
- Ridiculous!!

*Not Categorized*

- Connect north side cycle path at CIBC tower area, or Esso in Nottingham, with a pedestrian flyover to south side.
- Does not apply to us.
- We have lived in our home for over 40 years and the traffic increase (1) affects our east of access to Wye Road, timing of lights would help, (2) has caused a noise problem, possibly a fence in front of property would help (3) safety in turning into our property coming from west, suggest widening ditch area immediately for short distance, car length.

## 2.3 BRENTWOOD BOULEVARD INTERSECTION

Among the 41 individuals who responded to the statement about the Brentwood Boulevard intersection, 49% were satisfied and 51% were dissatisfied.



### *Very Satisfied*

No comments.

### *Somewhat Satisfied*

- Adequate.
- As long as lights allow enough time to cross. At the moment they do not.
- Concern for getting out of Estates Village safely.
- Filter lights needed at this junction.
- See comments in general section re jug handle encircling a future commercial/retail area.

### *Somewhat Dissatisfied*

- As above. However, the change to allow left turns further west as opposed to present problems on Wye and Brentwood seems good.
- Do not see the need to change this intersection.
- Poor access for residents of the Estates.
- Why three more sets of traffic lights between Brentwood and Sherwood Drive?



*Very Dissatisfied*

- Extensive road work and intrusion into residential neighbourhoods.
- Noise level!
- Ridiculous!!
- Traffic redirected from 233 to 232 - is this actually going to improve things overall?
- Very confusing to take this road after work done.
- We live in Estates Village and there is NO plans for noise control for the residents. This is an unbelievable omission for the added traffic on both Wye Road and RR 232. You are adding three lanes of traffic on land presently beside Estates Village fence and because of the gas right-of-way there will be little room for trees, bushes and sidewalks as well as some kind of control of the enormous noise increase. Our little retirement lovenest will be unbearable.
- Would like the phase light activated, going west on Wye Road, when turning south to RR 232.

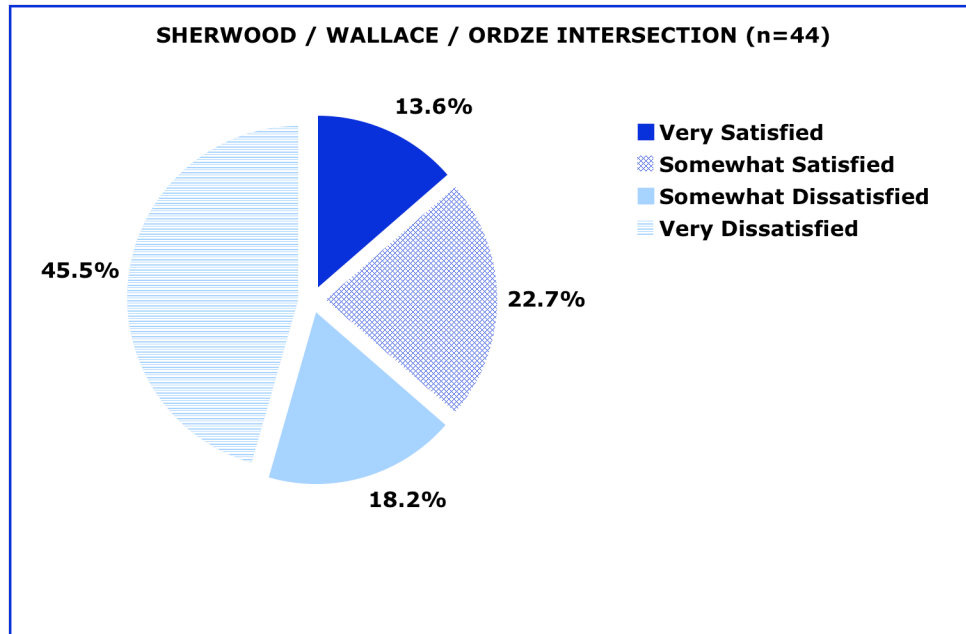
*Not Categorized*

- 1. Cannot get an answer on sound reduction for south side of Wye east of 232/Brentwood. 2. Cannot get answer on pedestrian movement from south trail to north trail near CBC tower.
- General comments. We are in a new age. The mindset of the driver needs to and will change. You have an opportunity to lead that, instead of catering to the old paradigm. In my opinion your sole focus is on traffic flow. You should be thinking about safety and community ambiance much more!
- Noise levels on south east corner.



## 2.4 SHERWOOD DRIVE / WALLACE DRIVE / ORDZE AVENUE INTERSECTION

Among the 41 individuals who responded to the statement about the Sherwood / Wallace / Ordze intersection, 36% were satisfied and 64% were dissatisfied.



### *Very Satisfied*

- Good planning.
- Great planning.

### *Somewhat Satisfied*

- Adequate.
- If possible, we would like a diagram or pdf of the Sherwood Drive/Wye Road proposal.
- Not a particular problem to us.
- Would only reduce some traffic from main intersection.

### *Somewhat Dissatisfied*

- A huge disruption to the area to speed up commuters a few minutes. The off-hours traffic is not a problem. This will satisfy the few hours per day of heavy traffic. Let them wait. Maybe they will be encouraged to take transit or car pool.
- As an owner of commercial property on Wye and Ordze, we need the flow of traffic on Ordze to keep clearing onto Wye quickly. Our only exit is on to Ordze.
- The noise level is going to be terrible. It is going from no traffic to crazy traffic.
- Too heavy traffic volume. Noisy. Poor business access.
- Yes, something has to be done about the congestion as it now stands, but adding

more stores and house density only compounds the problem. Think about the homeowners and businesses.

*Very Dissatisfied*

- All intersections and access drives need replacement. Removal of business and Chamber of Commerce will be necessary but not accounted for.
- Don't understand why conventional intersections could not function at these points (i.e. why the need for the jug handles?). It seems like a waste of taxpayer resources.
- Don't want all this extra traffic. Should be kept on high volume highway (Wye Road).
- Extremely dissatisfied!
- How do I access the back two-thirds of my property? Is there a gate through the fence and berm?
- Hurts local business too much for the minimal gain in traffic flow! Not enough planning has occurred to minimize negative impacts!
- Limits access to Ordze - makes it "unhandy". Divert consumers to Green Street (Green Street duplicates problems at Ordze and 233, how does that solve issue?)
- Loss of left turn south from Sherwood onto Wye Road is very inconvenient and forces drivers to go far out of their way. New road through Canadian Tire will be prohibitive to acquire land for no significant benefit.
- Needs to be a better understanding of traffic pattern along Alder Avenue, as a resident of this, how much traffic will be going along Alder after coming off Fir/Ash given traffic going south on Broadmoor can't turn left on to Wye? Don't count on everyone using the jug handle.
- No way do I want the road put in between Canadian Tire and Expert Lock.
- See above. The effect of this is to place major freeway through a community.
- See comments in general section - Wallace jug handle was approved prior to the open house, making the open house a public relations exercise in that respect. Turning right on Wallace to turn left later on Ash Street (which is not visible) is not a "natural" means of driving/human inclination, particularly if you can't see the point of exit/turn. Other parts of the plan are subject to future approval/funding which raises the possibility of half or less of an overall plan with undue impact on Wallace jug handle which may be questionable in any event - lack of details re construction of Wallace jug handle and measures to address concerns of adjacent residents/subdivision such as upholding established principles of physical/psychological separation of residential and commercial via a berm and sound barrier. There appeared to be some commitment in this regard but info from varying individuals was, at best, vague and at worst, contradictory. Commitment to a full berm plus sound barrier along the whole of Campbelltown Heights (on south side of the jug handle) to reflect what currently exists, and honour the commitments made during the Walmart development is required to offset in part the negative impacts upon the country residential subdivision of Campbelltown Heights.
- Walmart has had a negative impact on people's lives. It's no wonder those on Wallace Drive want to sell, their way of life has been ruined. Buying them out just passes the burden onto the landowners a little further down the road. I am vehemently opposed to the notion of extending the Wallace Drive jug handle west of 233 to then turn north by Canadian Tire. That would have a strongly adverse affect

on our little subdivision, Wye Road Gardens, and would most certainly decrease the property values and negatively impact our way of life.

- What confusion.
- What kind of engineering is it to not be able to turn left off RR 233 onto Wye Road - Sherwood Park Freeway - if your ridiculous plan is to left turn a block north?? This is actually turned into a jigsaw puzzle.
- Where did the traffic engineer get his training? Or did he get people in County office accept a nice payback gift?? There is no other explanation for this kind of engineering to even make sense. This is totally unrealistic! Anyone with common sense will at first glance tell you how unrealistic this plan is.
- Where is the berm we were promised by the developer and council? No public input again.

#### *Not Categorized*

- I feel for the businesses west of 233. Who will ever make such a convoluted journey from the east to go to the Dairy Queen or Earl's or Home Hardware? Those poor guys will be out of business?
- Must be able to walk out of Campbell Town Heights without getting hurt!
- No address of Wallace Drive extension to Ordze Park will severely strain the left turns off 233 (driving north) at Ordze, Ash and Fir (at London Drugs).
- Not apply.
- Put Wallace Drive through from RR 232 to Ordze Crescent Road and forget the jug handles.
- Roundabouts are not effective. Go to any country outside of Canada and see. They slow down traffic.
- Suggest flyover at Safeway lights or at least a pedestrian activated crossover.
- The ramp from Sherwood Drive south to London Drugs should be closed. The left turn between Fir Street and Wye Road on Sherwood Drive traveling south needs to be closing. Transit by bus needs to be available to all businesses in Sherwood Park. You should not need a vehicle to shop and recycle. With only room for one car in the left turn lane northbound from Ordze onto Wye Road west, the traffic will back up to block traffic on Ordze Road and Wallace Drive, a totally unacceptable corner - awkward and too short.

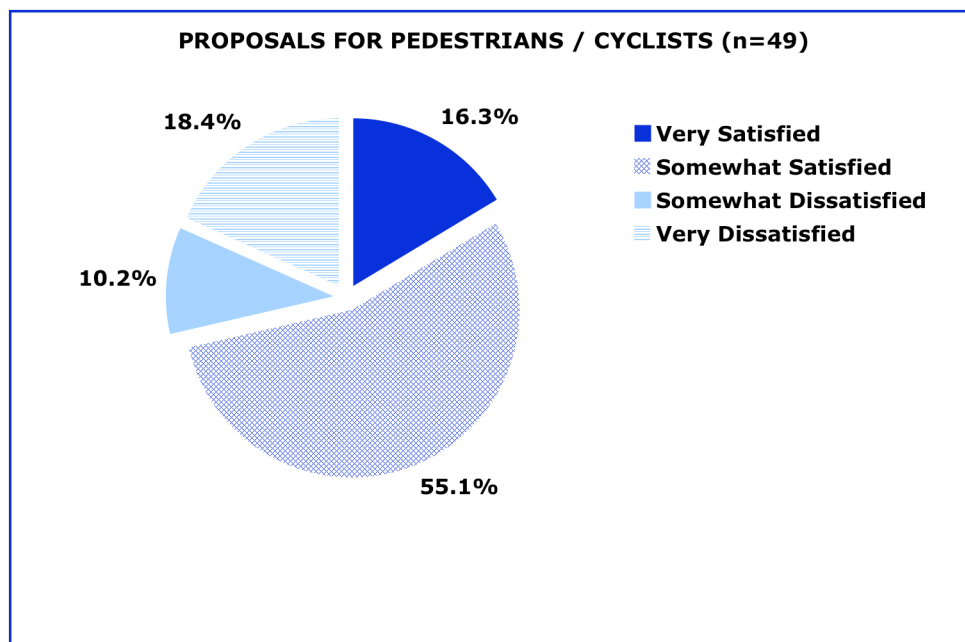
### 3. MODIFICATIONS FOR PEDESTRIANS AND CYCLISTS

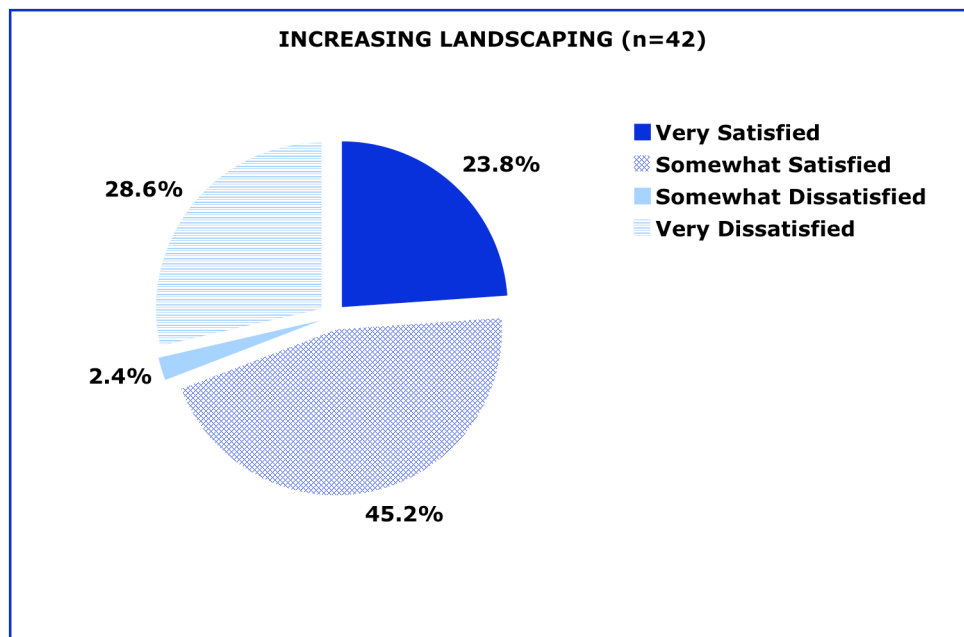
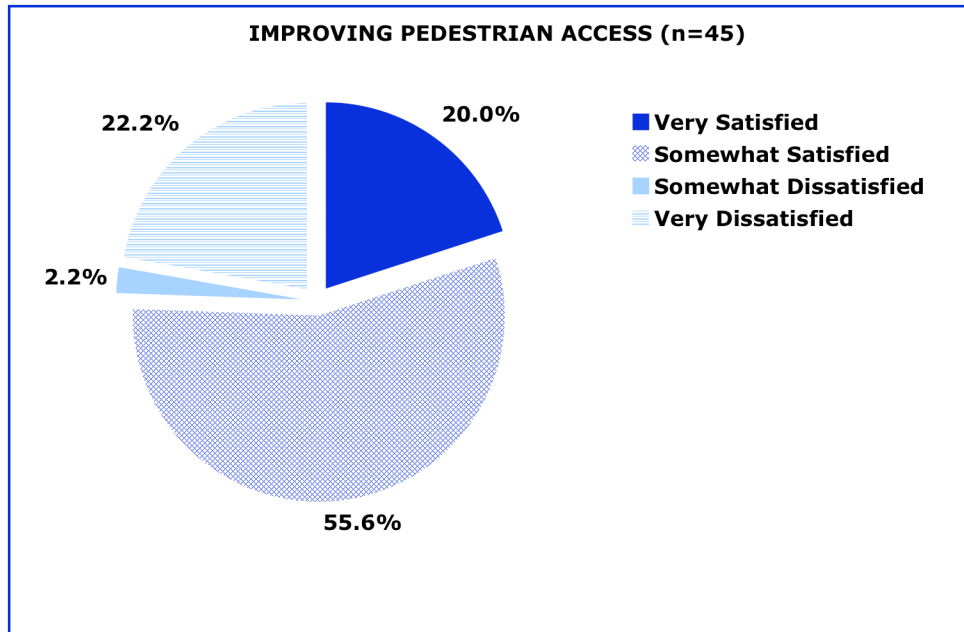
#### 3.1 Plan Proposals

With respect to the modifications suggested for pedestrians and cyclists:

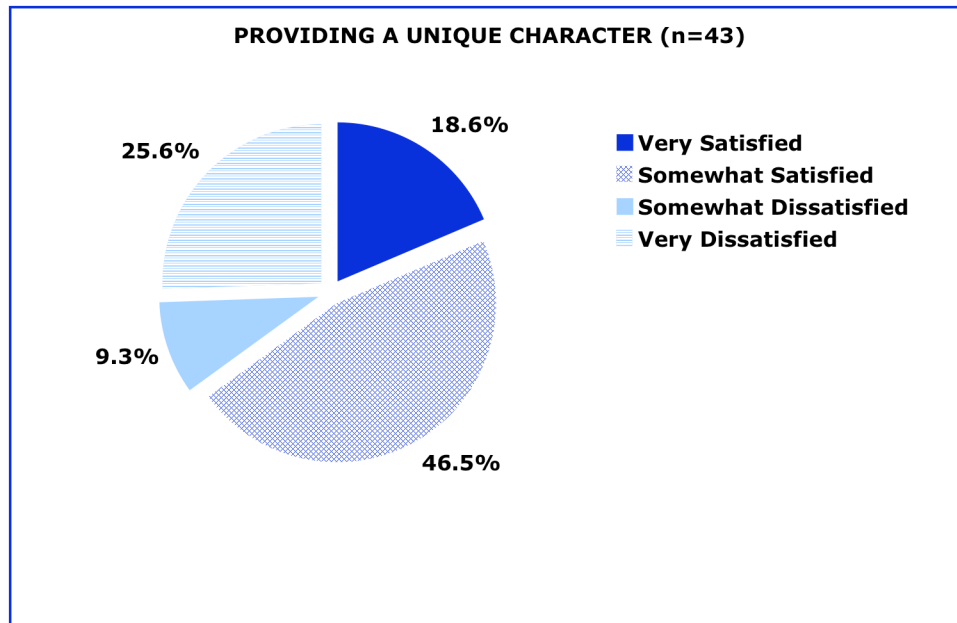
- 71% were satisfied and 29% were dissatisfied with the proposals overall;
- 76% were satisfied and 24% were dissatisfied with the proposals for improving pedestrian access;
- 69% were satisfied and 31% were dissatisfied with the proposals for increasing landscaping; and
- 65% were satisfied and 35% were dissatisfied with the proposals for providing a unique character.

Among the three option areas, the proposals related to improving access were viewed as positive by about three-quarters (76%) of respondents, with proposals for providing a unique character were slightly less important, with about three-quarters (65%) of respondents viewing them as positive.







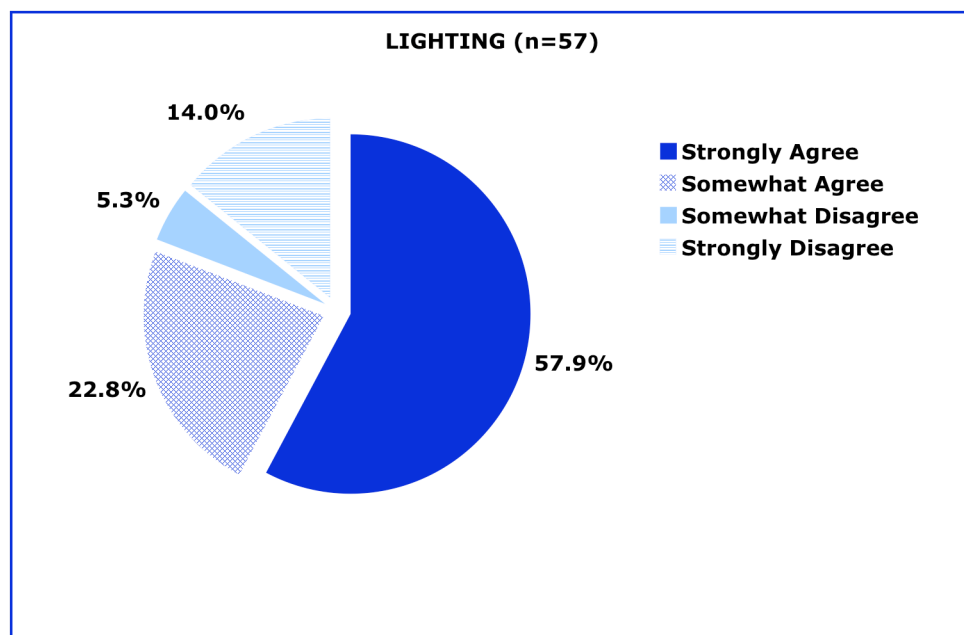
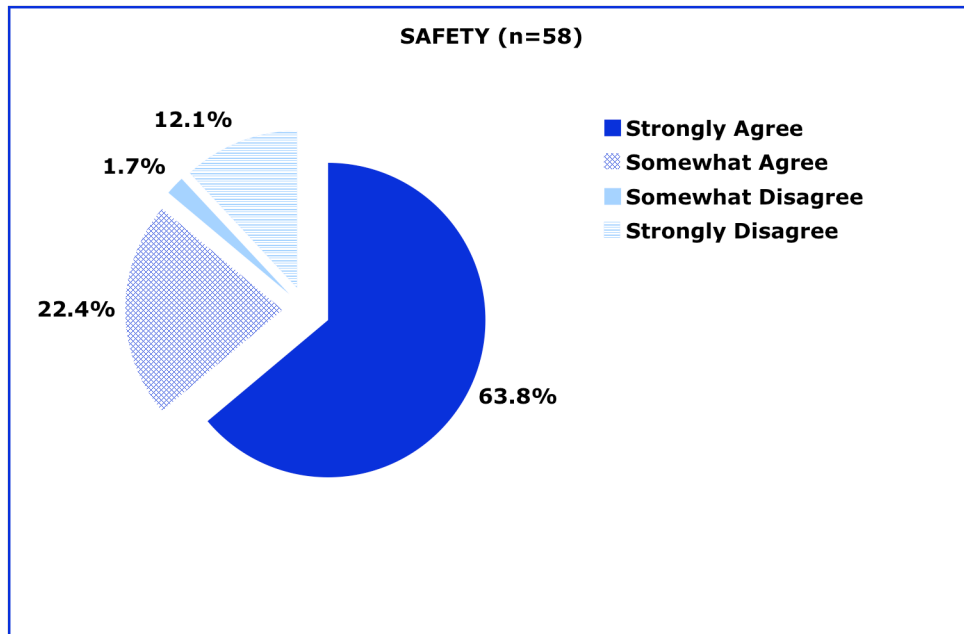


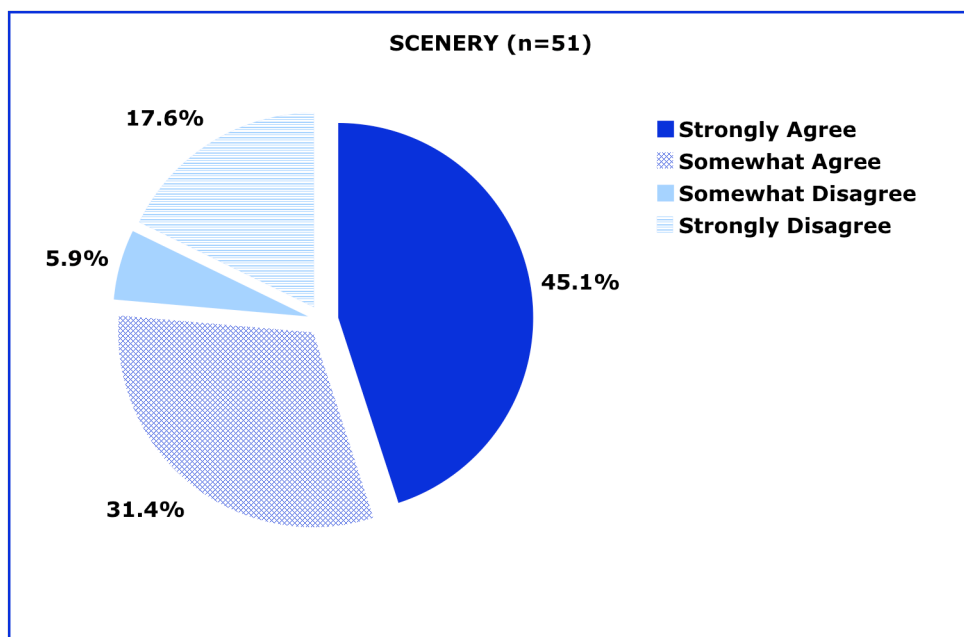
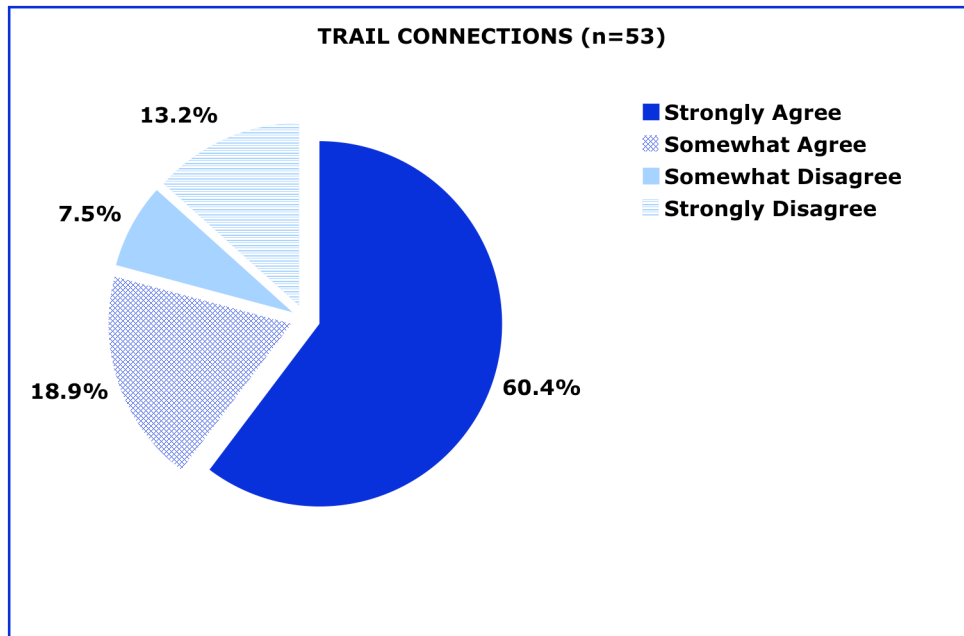
### 3.2 Encouraging Use

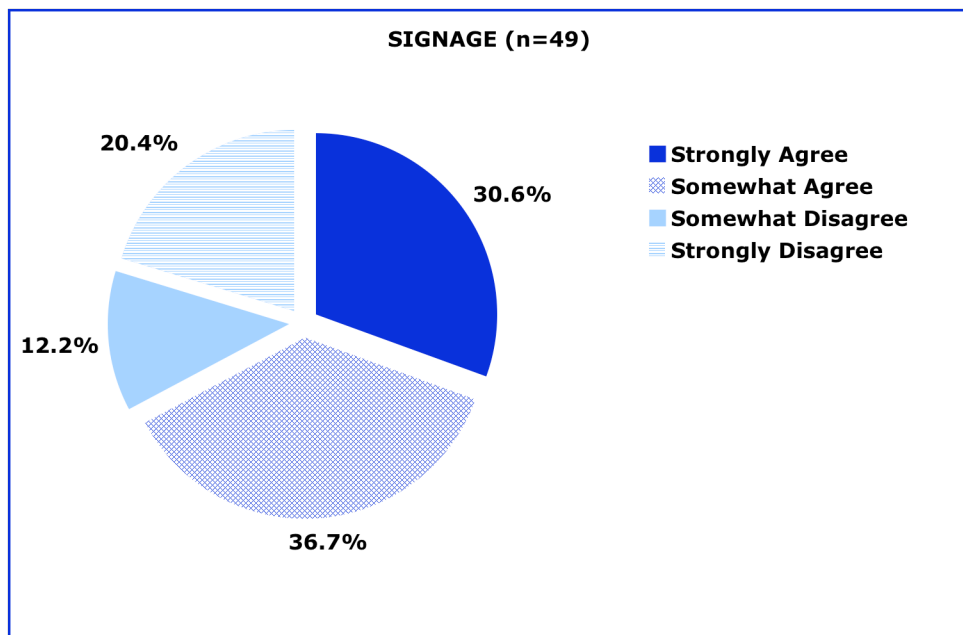
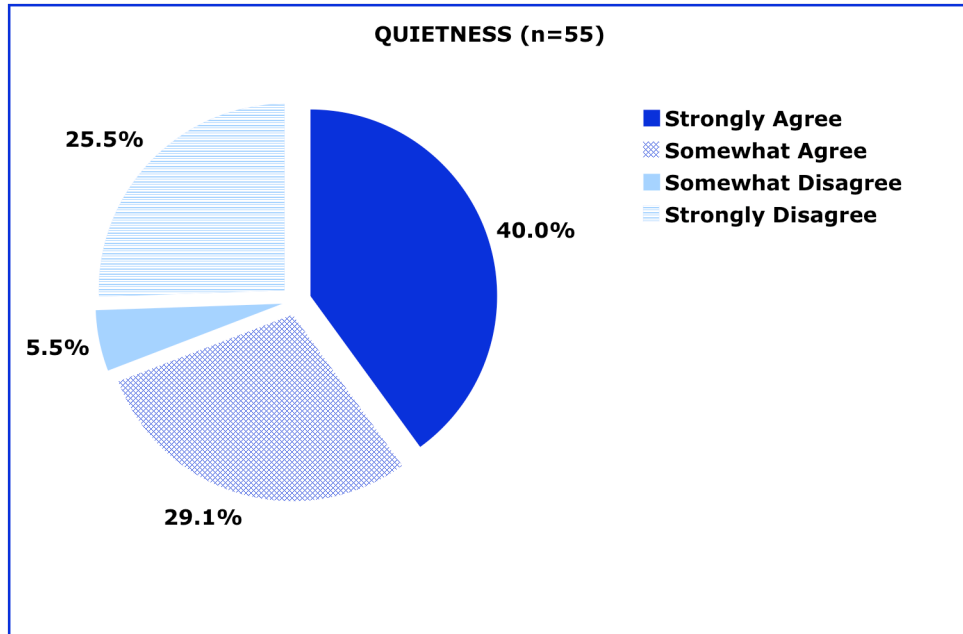
With respect to encouraging individuals to walk or cycle should the proposed developments proceed:

- 86% agreed and 14% disagreed that improved safety would encourage use;
- 81% agreed and 19% disagreed that improved lighting would encourage use.
- 79% agreed and 21% disagreed that improved trail connections would encourage use;
- 77% agreed and 23% disagreed that improved scenery would encourage use;
- 69% agreed and 31% disagreed that improved quietness would encourage use; and
- 67% agreed and 33% disagreed that improved signage would encourage use.

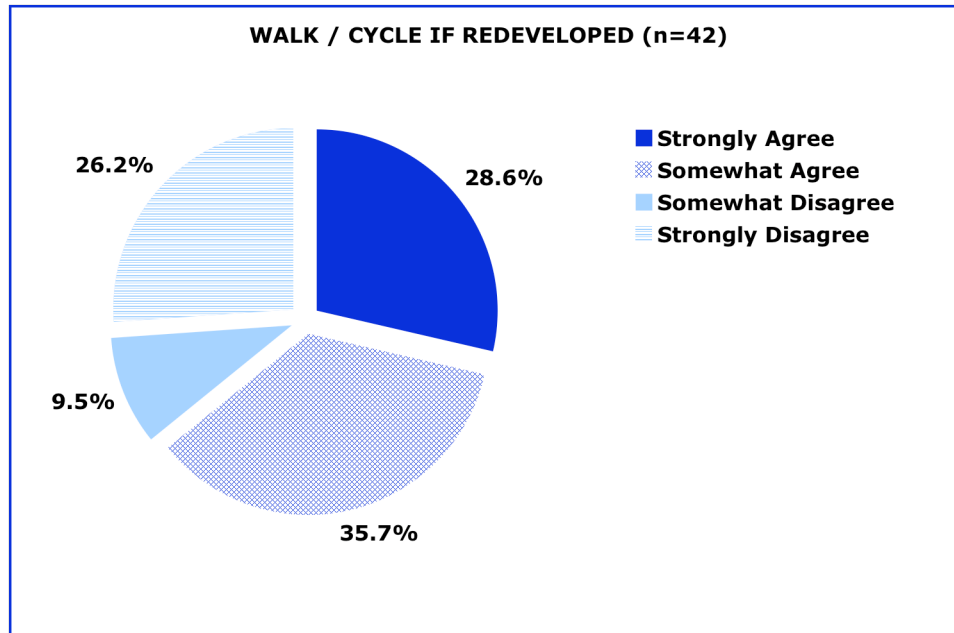
Improved safety (86%), including lighting (81%), were the factors most relevant to respondents being more likely to use Wye Road, with access by improved trail connections (79%) identified as the next most relevant factor. This latter result reflects the level of satisfaction with the suggestions proposed for improving trail access identified in Section 3.1. Improved signage, while indicated by about two-thirds (67%) of respondents, was considered important but the least influential factor in encouraging use.







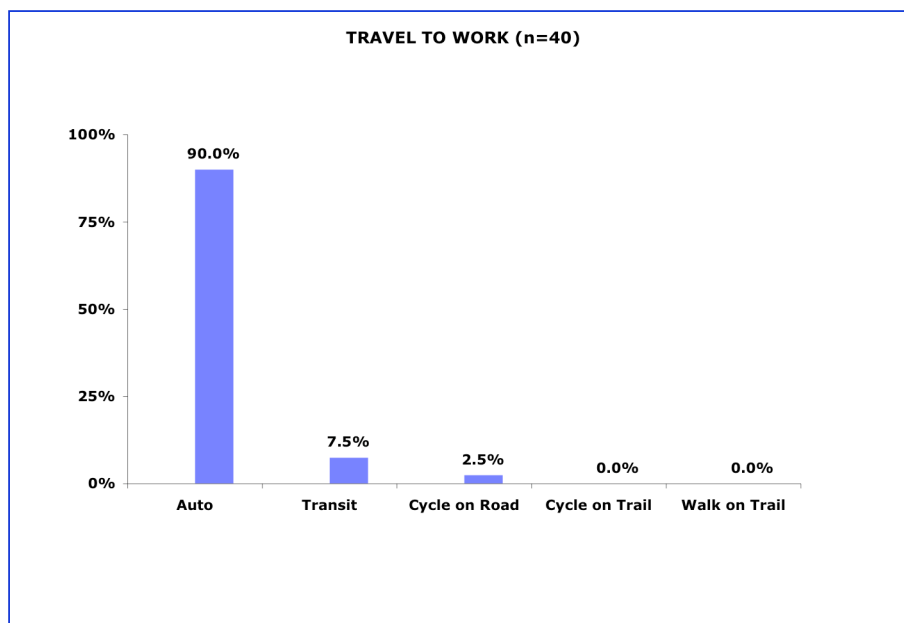
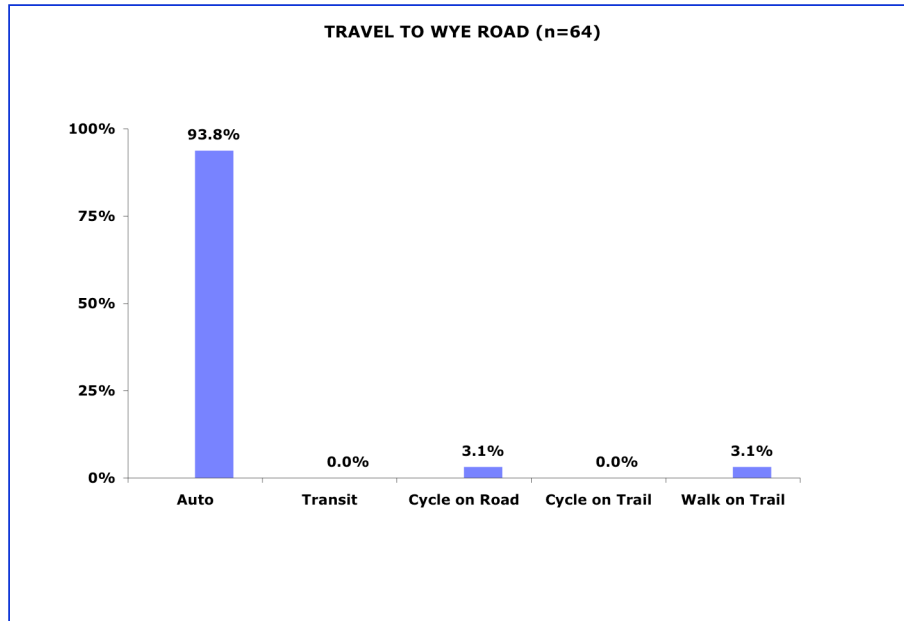
About two-thirds of respondents (64%) indicated they would walk or cycle to businesses and services on Wye Road if it was redeveloped using the proposed plan.

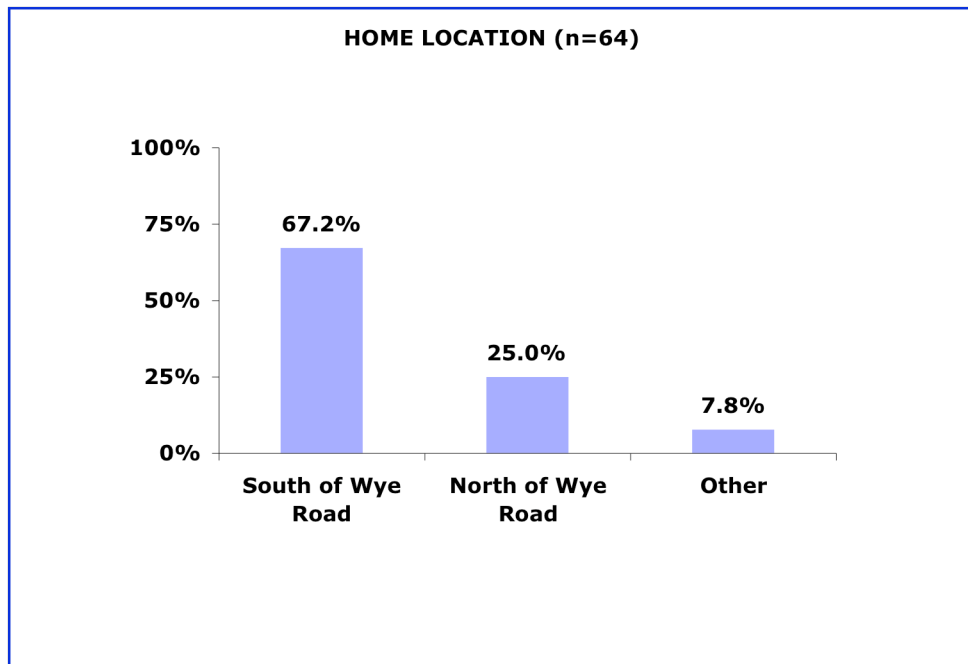
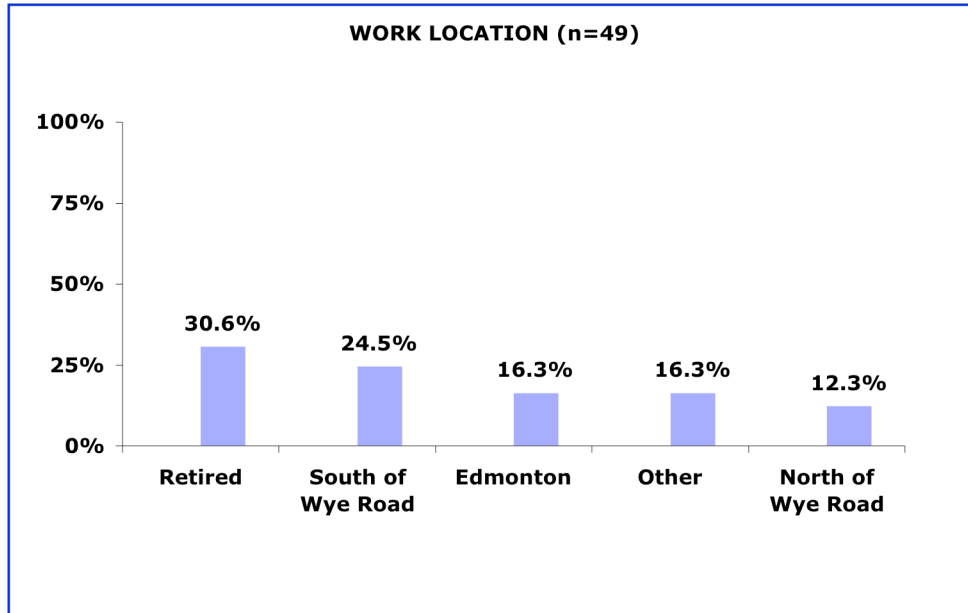


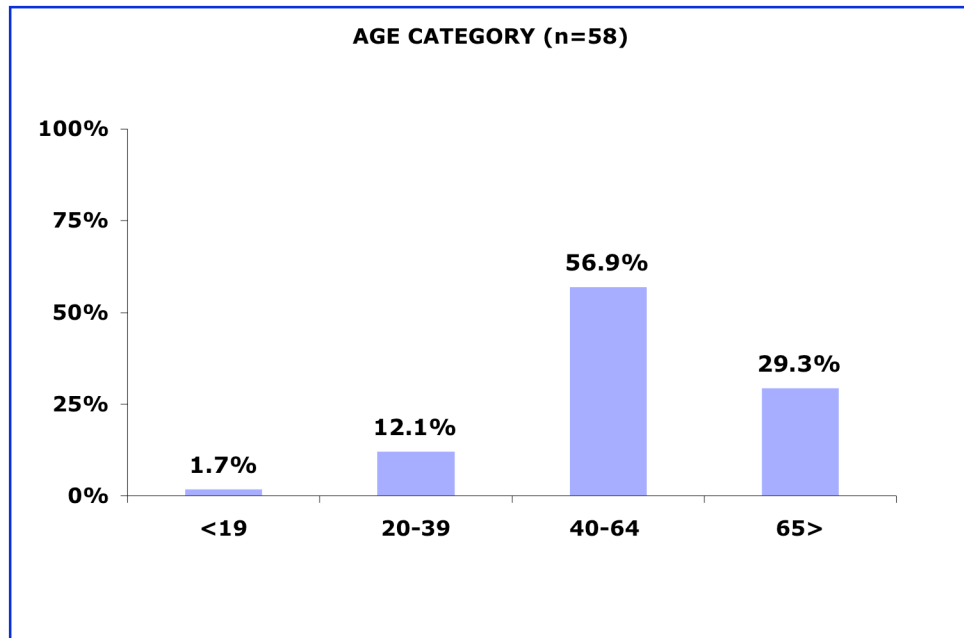


#### 4. CURRENT USE OF WYE ROAD

Virtually all of respondents (94%) indicated they most often use an automobile to travel to Wye Road services, at this time. Similarly, a large majority (90%) indicated they most often use an automobile to travel to work, with 8% taking transit. No respondents indicated that they cycle on trails to Wye Road services, or to work. While 16% of respondents indicated they work in Edmonton, and another 31% indicated they are retired, 25% work south of Wye Road and 12% work south of Wye Road. About two-thirds of respondents (67%) indicated they live south of Wye Road. Over half of respondents (57%) were aged 40-64 years, with another 29% being 65 years or older.

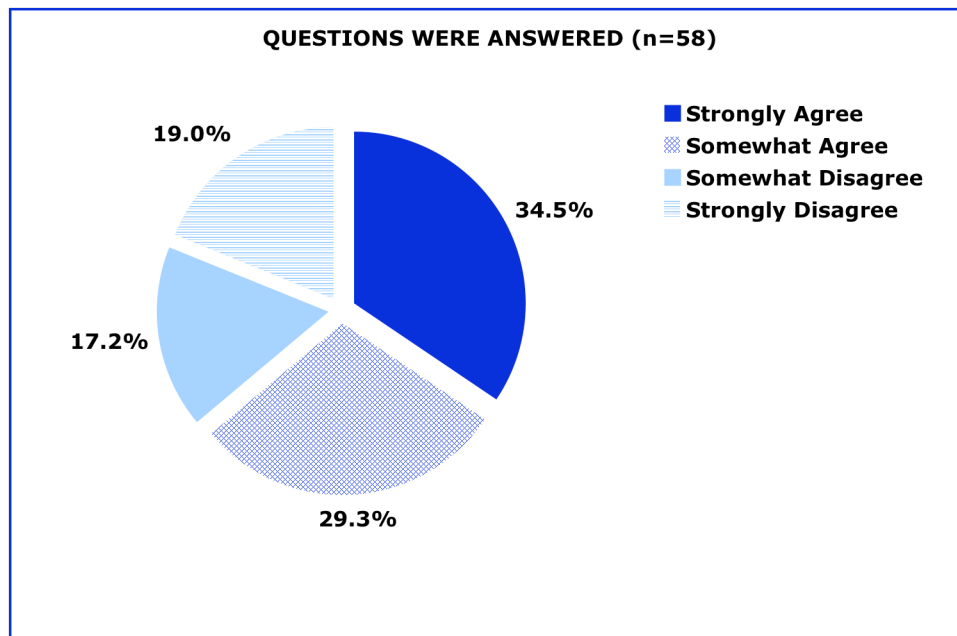
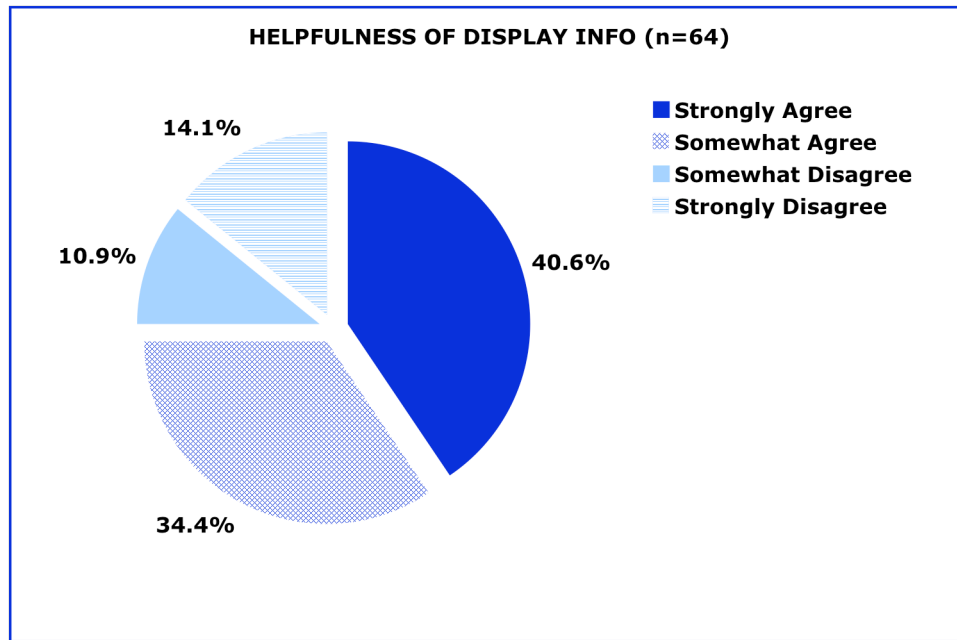






## 5. VALUE OF THE OPEN HOUSE

Attendees were asked to provide feedback on the helpfulness of the Open House. With respect to the helpfulness of the display information, 75% of respondents indicated that the information provided was helpful to them in understanding the plan, while 64% of respondents indicated that they felt their questions had been answered.



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**APPENDIX A  
COMMENT FORM**





### MODIFICATIONS TO ROADS AND INTERSECTIONS

**How satisfied are you with the proposals for the following?**

Very  
Satisfied

Somewhat  
Satisfied

Somewhat  
Dissatisfied

Very  
Dissatisfied

▪ **Overall**, the proposals in the Wye Road plan.

☐
☐
☐
☐

Please provide comments to explain your viewpoint.

▪ The **Clover Bar Road / Nottingham** area.

☐
☐
☐
☐

Please provide comments to explain your viewpoint.

▪ The **Brentwood Boulevard** intersection.

☐
☐
☐
☐

Please provide comments to explain your viewpoint.

▪ The **Sherwood Drive / Wallace Drive / Ordze Avenue** intersection.

☐
☐
☐
☐

Please provide comments to explain your viewpoint.

## MODIFICATIONS FOR PEDESTRIANS AND CYCLISTS

**How satisfied are you with the proposals for the following?**

	Very Satisfied	Somewhat Satisfied	Somewhat Dissatisfied	Very Dissatisfied
▪ <b>Overall</b> , the proposals for pedestrians/cyclists.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Improving <b>pedestrian access</b> .	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Providing a <b>unique character</b> .	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Increasing <b>landscaping</b> .	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Please provide comments to explain your viewpoint.				

**Would improvements to the following encourage you to walk and/or cycle along Wye Road?**

	Strongly Agree	Somewhat Agree	Somewhat Disagree	Strongly Disagree
▪ <i>Safety</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ <i>Quietness</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ <i>Trail Connections</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ <i>Scenery</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ <i>Signage</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ <i>Lighting</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ You would walk or cycle to businesses and facilities on Wye Road if it was redeveloped using the proposed plan and guidelines.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Tell us a little about you!**

	North of Wye Road	South of Wye Road	Other (please specify)	
▪ Where do you live?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
▪ Where do you work?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
▪ Which category includes your age?	<19 <input type="checkbox"/>	20-39 <input type="checkbox"/>	40-64 <input type="checkbox"/>	65+ <input type="checkbox"/>

**Getting around...**

	Auto	Transit	Cycle on Road	Cycle on Trail	Walk on Trail
▪ What method do you use <b>most often</b> to travel <b>to the businesses and facilities</b> on Wye Road?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ What method do you use <b>most often</b> to travel <b>to work</b> ?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Was this Open House helpful?**

	Strongly Agree	Somewhat Agree	Somewhat Disagree	Strongly Disagree
▪ The information on the displays gave you a good understanding of the proposed Wye Road plan.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Your questions were answered.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**PLEASE COMPLETE THIS FORM ... and leave it with us today ... or fax it to IMI strategics at 780-425-1423 by May 16.**

**THANK YOU**



## Appendix D.3

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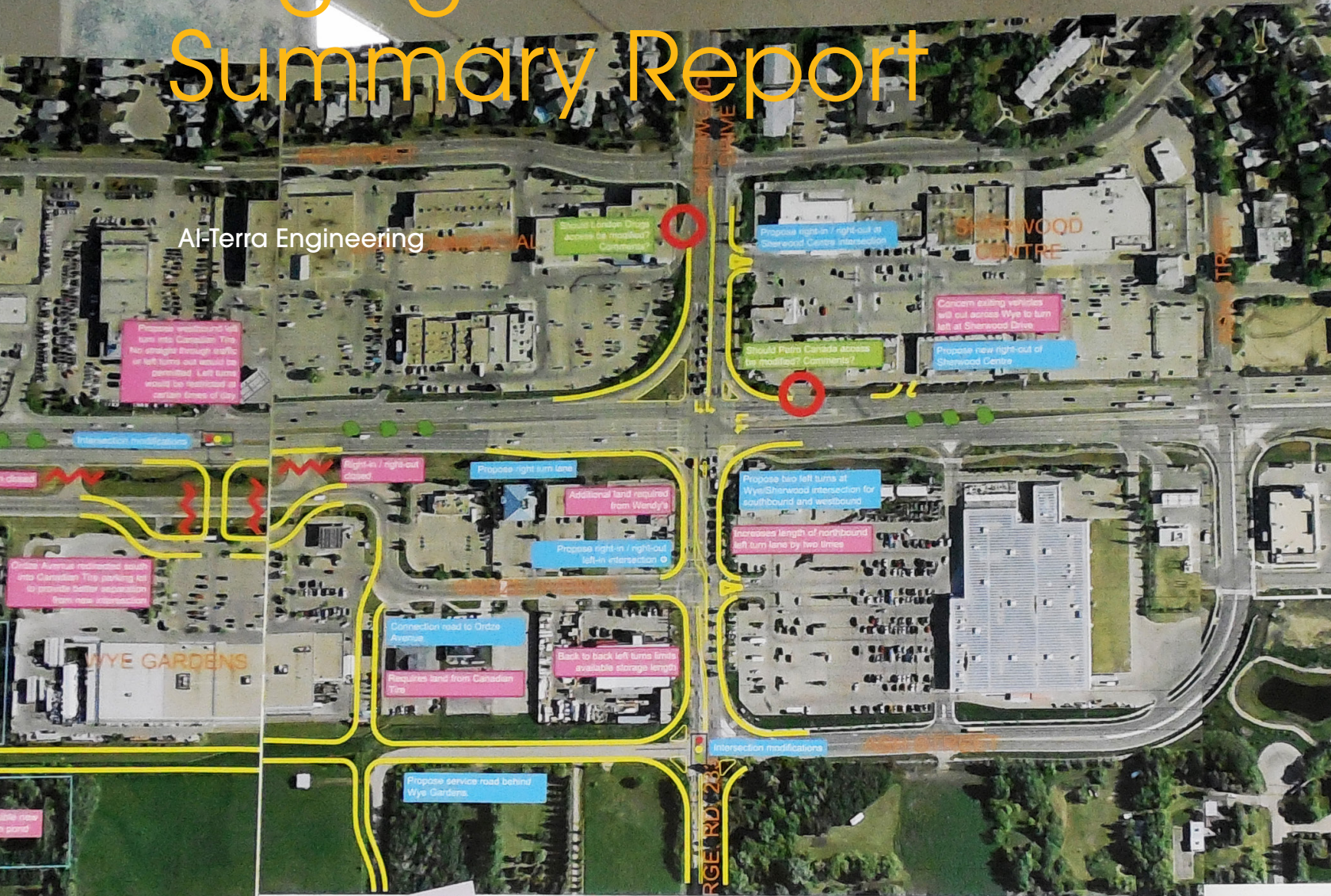
### Wye Road Stakeholder Engagement Summary Report (*Stantec*)





# Wye Road Stakeholder Engagement Summary Report

Al-Terra Engineering



ASH CROSSLAND WEST GOOD

CON-TIME ROAD IMPROVEMENTS CAN BE MADE. PLEASE

LOOKS GOOD! GUYS! i like it!

SHORT CAN TURN ACROSS EPO TO DRIVEY CROSSING NO LEFT TURN

REMOVE RS/FS AT WYE/ASH

OPTIONAL ACCESS FROM ASH STREET TO EARTH PARKING LOT?

REMOVE LEFT HAND TURN ON TO ORDOZ

INTERSECTION AT SHERWOOD AND ORDOZ - LOOKS OVERDUE

LOOKS MUCH BETTER FOR CONSIDERING YOUR FEEDBACK JOHNNY.

CONCERN CORNER AT CON-TIME EXPERTISE

NOT NEARBY - WILL AFFECT BUSINESS



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## Executive Summary

Strathcona County is committed to providing a safe and efficient transportation network for the community. In support of this, a functional planning study for Wye Road between Highway 216 and Highway 21 aims to address traffic congestion and safety issues along the corridor.

The Wye Road Functional Planning Study, initiated in 2006, was deferred in 2008 prior to completion based on concerns initiated by several businesses in the Wye Gardens commercial area. In September 2012, Council approved reactivation of the project based on a focused stakeholder engagement process with the objective of developing a formal, mutually acceptable plan for mitigating traffic congestion and safety issues at the intersection of Wye Road and Sherwood Drive and on Sherwood Drive immediately north and south of Wye Road.

The engagement process started in September 2012 and involved the business communities of Wye Gardens, Wye Commercial, Wye Crossing and Sherwood Centre. Numerous opportunities were made available to the stakeholders to participate and contribute to the stakeholder's process. The engagement process wrapped up in late June 2013 with a presentation of the stakeholder consensus recommendation. The final report summarizing the engagement process and outcome is attached.

### FIGURE 1 – PROJECT AREA

At the west end of the project, approximately between Ordze Road and Hawthorne Street, is the Wye Road Business Area, a well-developed business community comprised of privately owned shops, strip malls and box stores. Any proposed roadway network modifications along Wye Road in this area also need to attend to business access and viability concerns across the local stakeholder group comprised of property owners, property managers, and businesses.

Engaging stakeholders was fundamental in creating a plan for the Wye Road Business Area. This engagement plan was developed to provide every stakeholder with many opportunities for thorough input on the project. A number of methods were used to disseminate information and to effectively capture feedback. These included several personal face-to-face visits with stakeholders as a means to build personal relationships and foster honest feedback. A detailed master list of stakeholders and their history of involvement was used to keep track of the process.

Over the course of ten months, the Stantec engagement team conducted seven door-to-door visits with approximately 150 area stakeholders. Ninety-one stakeholders participated in the engagement process. By building personal relationships and employing varied engagement tools, stakeholders focused on design concepts and contributed thoughtfully to the Wye Road planning process.

The Stakeholder Engagement Plan had six separate phases, each of which provided a block on which to build a solid foundation for the next phase.

#### Phase I (September 2012) Project kick off

The Stantec team hand-delivered and/or mailed letters to each stakeholder to introduce the project. This face-to-face contact helped Stantec staff gauge the interest of the stakeholders regarding the project and to start building relationships. Stantec's team assured stakeholders that their input was important and that Strathcona County did not already have a plan in place. In Phase I, Stantec staff heard that safety and business accessibility were key concerns.

#### Phase II (October 2012) Distribution of information packages and questionnaires

Project information packages were hand-delivered to all the businesses informing them of upcoming engagement opportunities. The package included a short questionnaire inquiring about stakeholders' experiences, perspective, and comments on the Wye Road corridor. The response rate was a respectable 22%. This second round of face-to-face visits generated more discussion from the stakeholders. Stakeholders reiterated that safety and accessibility were key concerns. Retaining the left-hand turn on Wye Road to Sherwood Drive for westbound to southbound traffic was also important.

### Phase III (January and February, 2013) Face-to-face meetings

Stantec facilitated six engagement meetings hosted by a variety of stakeholders throughout the study area during Phase III of the process. Stakeholders were assured that this process was a fresh start this part of to the functional planning project. They were also told that there was a genuine desire on behalf of Al-Terra and Strathcona County to collaborate with the stakeholders to determine viable options for roadway improvements based on stakeholder recommendations. At the meetings, stakeholders learned more about the current transportation issues within the study area as well as potential solutions. The meetings provided an opportunity for stakeholders to collaborate with each other and the Stantec engagement team in a meaningful and open way. Stakeholders provided their own visions of how they would like the plan to develop.

The participants stated that safety and accessibility were important for all users of the area (employees, business owners, shoppers, etc.) and for the different modes of transportation that brought those people to Wye Road (walking, cycling, driving, etc.). The stakeholders also felt that it was important to pay attention to the aesthetics of the area.

One of the outcomes of the meetings was a variety of “tools” developed by the stakeholders for roadway network improvement. These tools included intersection and roadway widening, new access locations, access closures, adjusting access configurations, review of signal timing, and additional landscape planting amongst others. A complete list of the tools generated is included in Appendix IV.

### Phase IV (March 2013) Stakeholder feedback sessions

Two feedback sessions were held for stakeholders to further collaborate and evaluate the input of others. Using visual representations of Wye Road, stakeholders evaluated and ranked 22 tools that had emerged from Phase III. Preferred tools (Appendix IV) started to emerge from the stakeholder input, resulting in a list of preferences, notably:

- Tool 1A – Support and conditional support of signal timing improvements.
- Tool 1C – Support for a new limited access signalized intersection on Wye Road providing access to Canadian Tire, Midas, Park Mazda, etc.
- Tool 2F – Support for signals at the intersection of Sherwood Drive and Ash Street.
- Tool 2A – Support for widening Ordze Road.

## Phase V (May 2013)

### Design concepts

The stakeholders' input was used to develop three potential design concepts. The amount of modification required within the study area to implement the concepts ranged from slight (Concept 1) to substantial (Concept 3) (Appendix V). The concepts were reviewed by a professional transportation engineer for viability prior to presentation at the session. It was apparent that three stakeholders' properties and onsite operations would be particularly impacted by the design Concepts 2 and 3. Stantec met with each of these stakeholders. The three potential concepts were introduced to the larger group at two different meetings. Stakeholders ranked their preferences. Concept 2 was clearly preferred by the larger group of stakeholders.

## Phase VI (June 2013) Final Recommendations

Phase VI consisted of reviewing all of the stakeholder input and developing a plan with the final stakeholder recommendations. This plan was presented at two stakeholder meetings. The majority of participants were positive and supportive of the recommendations. Stakeholders agreed that their opinions were considered and a viable compromise was achieved.

Please refer to page 41 to see the final recommended plan.

## 1.0 Overview

### 1.1 Project Understanding & Approach

#### 1.1.1 Project Background

Strathcona County, supported by Mayor and Council, follows transportation engineering principles, guidelines and best practices for the provision and operation of roadways in the County.

In 2006, Strathcona County hired Al-Terra Engineering to conduct a Functional Planning Study (FPS) for Wye Road between Highway 21 and Highway 216. The focus of that study was to address roadway congestion and safety issues due to the growth in Sherwood Park and the related increase in traffic volumes. The study's recommendations for long term improvements would constitute the development of a plan to accommodate future growth and to mitigate existing and anticipated operational deficiencies along the corridor.

The section of Wye Road from Ordze Road to Hawthorne Street was included in the 2006 FPS, but the recommendations for roadway improvements made in the report were not supported by the local area property owners, property managers, business owners and business managers when presented at the FPS stakeholder information meeting in 2008. At that time, the stakeholders expressed concern that some of the proposed upgrades might not be suitable and could be detrimental to their businesses. Without clear consensus on how best to address the stakeholder concerns, the FPS was never finalized.

#### 1.1.2 Existing and Future Traffic Concerns

In 2006 the County identified several Wye Road intersections as having operational concerns from both a traffic safety and traffic capacity perspective. There are also other safety concerns and considerable delays in traffic. County data from 2012 indicates that the Wye Road/Sherwood Drive intersection is ranked fourth for collisions.



Future<sup>1</sup> traffic volumes on Wye Road are anticipated to be similar to current traffic volumes on Baseline Road. As growth continues and traffic volumes increase on Wye Road, safety, congestion, and traffic delays will continue to be issues which the County will need to monitor and address.

### 1.1.3 Current Conditions

The Wye Road corridor has changed significantly since 2006. Wye Road Crossing and Salisbury Village Commercial Centre are now developed. Sherwood Centre has been redeveloped. Ash Street behind the Wal-Mart Site was constructed and Wye Road was widened between Sherwood Drive and Hawthorne Street. These modifications to the road network align with the 2006 Functional Planning Study. Growth along the corridor will continue so it is necessary to have a Council approved plan for Wye Road to ensure adjacent development continues to align with the plan for the long term road network. Timely roadway upgrades will be implemented in a manner that will minimize traffic congestion for the motoring public while maintaining safe and efficient access to the different business precincts as well as neighbouring residential communities.

In 2012 Strathcona County authorized Al-Terra to plan and implement a stakeholder engagement process focusing on the Ordze Road-Hawthorne Street business corridor of Wye Road. The goal of the engagement process was to clearly identify stakeholders' issues and develop acceptable potential solutions in order to complete the FPS. Al-Terra retained Stantec Consulting to undertake the stakeholder engagement process. This stakeholder engagement study area focused on businesses and properties between Ordze Road and Hawthorne Street from Fir Street to Ash Street (Figure 1).

### 1.1.4 Project Objectives

The purpose of the public engagement process was to gather input from the property owners, property managers, business owners, and business managers in the area (hereafter known as the stakeholders) on the perceived issues, to involve them in defining acceptable options for traffic management and to create a stakeholder recommended plan which could be evaluated for feasibility, cost benefit, and safety.

Safe and efficient movement of traffic on Wye Road were the core considerations.

Although it would be preferable for stakeholders to reach consensus on the plan, it was acknowledged that this was unlikely to occur. The public engagement process was designed to build toward a majority of stakeholders accepting a plan that generally fulfills the objectives of improved traffic movements, improved safety, and continued business accessibility.

<sup>1</sup>“ Future” volumes refer to expected traffic volumes when Sherwood Park has no more vacant developable land within its current boundaries

The recommended plan will be incorporated into the overall FPS for Wye Road and presented to County Council for consideration and approval. Roadway upgrades will be prioritized in the FPS, which will include implementation triggers based on operational and safety thresholds.

#### 1.1.5 Geographic Scope of Project

The project focused on engaging stakeholders (described previously) located on Wye Road between Ordze Avenue and Hawthorne Street as well as adjacent owners north and south of Wye Road between Fir Street and Ash Street (Figure 1). Although the focus of the engagement was to generate options and ideas about what changes could be made to Wye Road, streets do not operate in isolation, and adjacent roads and intersections can have a significant impact. Therefore, along with the intersections on Wye Road, the following intersections were identified as potential sites for improvements and upgrades:

- Fir Street/Sherwood Drive
- Ordze Avenue/Sherwood Drive
- Ordze Avenue/Ordze Road
- Ordze Road/Fir Street
- Other entrance/exit points along Wye Road

Stakeholders further identified the section of Sherwood Drive between Fir Street and Ash Street as being problematic, stemming primarily from the accesses into the Sherwood Center and the Wal-Mart Site complex.

### 1.1.6 Process Methodology

Stakeholders had multiple opportunities to participate during the 6 different phases. This iterative approach built trust and momentum with the stakeholders to progressively focus ideas and options for corridor modifications.

#### **Phase I – Project kick off**

An introductory project letter was mailed and/or hand delivered to every business within the scope of the project.

#### **Phase II – Distribution of information packages and questionnaires**

Stantec staff visited with as many stakeholders as possible to further describe the project objectives, timelines and the engagement plans. Stakeholders received a questionnaire and an information package.

#### **Phase III – Face-to-face meetings**

Informal stakeholder engagement meetings were held at local businesses. Stakeholders were encouraged to provide their ideas for “tools” on how the Wye Road corridor and their businesses could be enhanced.

#### **Phase IV – Stakeholder feedback sessions**

A second round of stakeholder engagement meetings was held to discuss the ideas generated from Phase III. Pictorial representations of the “tools” that were developed in Phase III were displayed. A mapping exercise allowed stakeholders to review and provide feedback on the specific tools.

#### **Phase V – Design Concepts**

Stakeholders reviewed and provided feedback on the desired concept components from Phase IV. This resulted in three different conceptual plans. Another mapping exercise was conducted to determine the stakeholders’ preferred concept.

#### **Phase VI – Final Recommendation**

A final series of stakeholder engagement meetings were held to gather stakeholders’ final commentary on the recommended design.

## 2.0 Stakeholder Engagement Process

Stantec created a master stakeholder engagement database by combining records from the County with those from the Sherwood Park Chamber of Commerce. The database was used to track all contact with each stakeholder. The database was actively updated during each phase of the project to ensure every business had as many opportunities as possible to be engaged. The database included stakeholders' contact information, their preferred method of contact, the cumulative history of their participation and their input throughout the life of the project (Appendix VII).

Stakeholders were made aware of opportunities for input via email and by visits from Stantec staff over the life of the project. Each stakeholder had access to the project information whether or not they participated in the process.



## 3.0 Phase I – Project Kick Off (September 2012)

### 3.1 Phase I Objectives

There were two main objectives for Phase I: create relationships and build trust with stakeholders; and confirm the accuracy of the stakeholder database and add new information as it became available.

### 3.2 Phase I Implementation

To initiate the project, each stakeholder (about 150) received a hand-delivered introductory letter on Al-Terra letterhead (1<sup>st</sup> site visit) between September 17 - 20, 2012. The introductory letter was also mailed to the property owners. The letter consisted of project information, contact information for the Stantec engagement team, and information regarding the upcoming phases of engagement (Appendix I). Key discussion points during the door-to-door visits were:

- Project objectives and timelines
- An outline of the study area
- Brief project history
- The proposed engagement plan
- Collecting accurate contact information.

This gave Stantec an opportunity to build relationships with the stakeholders as well as identify and manage any concerns and negative perspectives. Business cards were collected and the data base was updated.

### 3.3 Phase I Results

It was noted that there were stakeholders who felt very passionate about their property/business. There was also mistrust of the County, the process and the project overall. Stantec heard stakeholders say that “the decision has already been made” and “the County already has a plan which they will enforce”. Once stakeholders were reassured that the County really did want to engage them collaboratively they were generally pleased to have the opportunity to voice their concerns and to take part in the project.

### 3.4 Phase I Contribution to the Project

Phase I confirmed the importance of one-on-one engagement in order to build trust and develop relationships with stakeholders. The creation of the database helped to minimize gaps and to track the input received from the large stakeholder group. Phase I provided a solid project foundation for the remainder of the project.

## 4.0 Phase II – Handout Packages and Questionnaire Distribution (October 2012)

### 4.1 Phase II Objectives

Similar to Phase I, relationship building and refinement of the master stakeholder engagement database were key objectives. Critical to this phase was the determination of who would want to participate in Phase III's local stakeholder's meetings.

Again, efforts were made to ensure all the stakeholders were aware of the project and the opportunities available to provide input into the collaborative engagement process.

### 4.2 Phase II Implementation

Approximately one month after Phase I (October 24 and 25, 2012) a letter and a more detailed project information package (Appendix II) were delivered (by hand and/or email) to every business in the project area. This package included:

- An overview of the engagement process outlining the various opportunities for stakeholders to participate.
- A map outlining the project area to resolve any confusion with construction for the Anthony Henday Interchange at Wye Road.
- A short questionnaire along with information for its convenient return, including a pre-addressed and postage paid envelope (Appendix II).

The questionnaire gathered information on stakeholders' own experience working, driving and walking in the project area as well as any other additional feedback. Stakeholders were also asked if they would be interested in attending a meeting with other stakeholders in the area.

### 4.3 Phase II Results

Stakeholders started to recognize the Stantec team members during this phase of the project. People were asked once again if there were other stakeholders who should be involved. The database was updated.

Thirty-three questionnaires were returned (a respectable 22% response rate). Twelve people indicated they would like to participate in the local stakeholder meetings, sixteen declined and five were unsure. Their compiled responses can be found in Appendix II.



Highlighted comments from the questionnaire:

- Stakeholders expressed concern with the prior plan (circa 2008) as being the final design, making the current process a waste of time.
- Stakeholders' views regarding traffic safety were polarized. Some saw it as a major concern and others felt it was not significant.
- It was important to stakeholders that accessibility to businesses by all transportation modes be considered in the process.
- There was strong opposition to the removal of the left turn on Wye Road (westbound) at Sherwood Drive.
- Stakeholders agreed that access to the Park Mazda, Midas, etc. sites is problematic.
- Beautification of Wye Road would be appreciated by stakeholders.
- Signal timing improvements for intersections within project area would be a big improvement.

#### 4.4 Phase II Contribution to the Project

The feedback from the questionnaires confirmed stakeholders' important issues and gave insight into their experiences on the Wye Road corridor. This was useful for developing a framework for the later phases of the process. The consistent one-on-one contact with the stakeholders confirmed the County's commitment to a collaborative process.

It became clear that Phase III needed to be postponed from mid-November to mid-January to be effective and to accommodate as many stakeholders as possible as the holiday season is a busy time for most of them. Stakeholders were contacted and advised of the change in timelines.

## 5.0 Phase III – Face-to-face Meetings (January - February 2013)

Phase III provided an opportunity for the stakeholders to collaborate in the development of a functional roadway plan for the study area. The Stantec team was sensitive to stakeholders' vested interests. The face-to face meetings provided a comfortable and open venue for the participants to voice their concerns as well as to hear each other's perspectives. The collaborative nature of the meetings provided an atmosphere of problem solving.

### 5.1 Phase III Objectives

The main objective of this phase was to provide an opportunity for stakeholders to talk to each other to learn about others' experiences, share their concerns, discuss potential solutions and consider trade-offs.

### 5.2 Phase III Implementation

Due to Phase III being postponed from November to January, it was prudent to contact each stakeholder reminding them of the change. They were contacted by email, by phone or at their place of business (3<sup>rd</sup> site visit). When contact was made, Stantec staff confirmed receipt of the project information, provided information about the upcoming local stakeholder engagement meetings, and answered any questions.

The local stakeholder meeting format was designed to allow for open, frank discussion regarding current transportation issues within the study area affecting the stakeholders and potential solutions.

Each meeting started with a brief introduction and overview of the project (Appendix III), and affirmed the County's desire for a collaborative approach. Stakeholders were assured that there was no current plan awaiting ratification and the design concept would be based on stakeholders' recommendations. Background information was summarized and next steps were reviewed. Participants learned about current traffic volumes, traffic safety, and potential growth on the Wye Road corridor.

Stakeholders were asked to reflect on the growth in the area and in Sherwood Park over the past five, ten, and twenty years. Then they considered what the same level of growth would look like in 2033. Participants thought about traffic safety, traffic volumes, growth, and the opportunity to enhance business viability and how various traffic management tools could help. Impacts at the various locations and along the transportation corridor were contemplated.

The meeting culminated with people reviewing a large aerial photo of Wye Road. Stakeholders drew on it to reflect the suggestions, information, concerns, and ideas discussed at the meeting. This gave them opportunity to see what the various techniques might look like. The participants were encouraged to develop concepts using the “complete streets approach” to community planning. “Complete streets are streets for everyone. They are designed and operated to enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work” (National Complete Streets Coalition).

Stakeholders were encouraged to consider how they wanted the Wye Road ‘neighbourhood’ to look, feel, and function.

### 5.3 Phase III Results

Six meetings were held from January 28 – February 5, 2013 at times most convenient to the stakeholders. Twenty-six participants representing twenty-four businesses/properties attended. Mayor Osinchuk and Councillors Delainey and Bidzinski attended the meeting on January 28, 2013.

Stakeholder discussions generated tools they felt could address traffic congestion and access issues in the project area.

A thirteen page summary was compiled (Appendix III). Most of the comments were general and applied to the overall project area. Themes and values emerged which are summarized in the table below. Other comments were specific to particular roadways and/or intersections. The summary of the stakeholder session was provided to each stakeholder in the project area for their review and information.

Common themes:

- Access
- Bicyclists and pedestrians
- Drainage
- Intersection design
- Landscaping
- Parking
- Safety

Key values:

- Accessibility
- Accommodate all users (pedestrian and bicycle)
- Enhance business viability
- Overall safety

Other stakeholder comments included:

- Build a connecting road Wye Road to Fir Street through Park Mazda's parking lot.
- Redesign the Sherwood Drive access to the Save-On-Foods site to increase safety.
- Redesign the Ordze Avenue and Sherwood Drive intersection to increase safety.
- Retain the left turns on Wye Road (westbound) at Sherwood Drive.
- Reduce traffic congestion on the corridor. This will become increasingly important as growth continues.
- Reduce collisions on the corridor. Collisions during peak times can detrimentally affect access to the surrounding businesses.

#### 5.4 Phase III Contribution to the Project

Phase III brought stakeholders together to develop general themes and common values while providing the framework for Phase IV. Phase III was a critical step in the process in that stakeholders identified the building blocks that could be used to develop concepts. They appreciated the opportunity to develop concepts that represented their solutions, fostering acceptance of the engagement process.

## 6.0 Phase IV – Stakeholder Feedback Sessions (March 2013)

Stakeholders continued to collaborate to develop a functional roadway plan for the study area in Phase IV. Two stakeholder engagement sessions were held to provide a summary of the input received so far as well as exhibit the many tools which had been suggested by the stakeholders. The large display made it easy for participants to review the results of the Phase III meetings.

### 6.1 Phase IV Objectives

The intent for Phase IV was to engage the stakeholders to consider and refine the tools and ideas tendered up to this point, as well as encourage further ideas. Phase IV also intended to reach out to new stakeholders who had not participated previously.

### 6.2 Phase IV Implementation

Invitations and the summary document were hand delivered (4<sup>th</sup> site visit) and/or emailed to stakeholders. The invitation advertised the two drop-in stakeholder sessions, held on Thursday, March 7 and Tuesday, March 12 from 3 pm – 7 pm at the Sherwood Park Chamber of Commerce. Thursday's meeting was not well attended so Stantec's team decided to offer a draw prize of a ¼ page advertisement (\$525 value) in the local newspaper as an incentive to increase attendance at Tuesday's meeting. The stakeholders were notified of the second open house and the draw by a 5<sup>th</sup> door-to-door site visit and via email.

Participants entered the room to see large display boards that summarized the values, themes and the various tools developed by stakeholders in previous meetings. Twenty-two tools were presented along with a list of their respective benefits/ advantages and challenges for implementation. Participants were given one sticky dot for each of the 22 tools to indicate their level of support by placing the dot under the titles "Fully Support", or "Support with Conditions", or "Not Support". Participants were encouraged to explain their choices by writing in the comment section. This "discussion board" provided participants with a certain amount of anonymity. By the end of the meeting, it was easy to see which tools were more desirable than others. This information was compiled into a report for use in the Phase V.

### 6.3 Phase IV Results

Twenty-one participants representing fourteen businesses and properties attended the meetings. Councillors Delainey and Bidzinski attended the Thursday stakeholder session. Seven stakeholders participated for the first time in the project.

The input received was summarized including the level of support for each tool and the associated comments. (Appendix IV). Of particular note are the following:

- Tool 1A - Support and conditional support of signal timing improvements.
- Tool 1C - Support for a new limited access signalized intersection on Wye Road providing access to Canadian Tire, Midas, Park Mazda, etc.
- Tool 2F – Support for signals at the intersection of Sherwood Drive and Ash Street.
- Tool A – Support for widening Ordze Road.

More detail is provided in Appendix IV:

### 6.4 Phase IV Contribution to the Project

Stakeholder input helped to refine and prioritize the tools for consideration in Phase V. A summary of the results was distributed to each stakeholder to apprise them of progress made to date and the tools that were being considered for the next phase (Appendix IV).

Several new stakeholders provided input. All stakeholders received project information and were up-to-date with the progress of the project.

Rather than exhibiting just one plan at the next engagement session, the team determined that three concepts should be compiled and displayed for stakeholder comment prior to creation of a final recommended plan. This approach resulted in an additional project phase, but it was generally felt that the inclusion of an additional phase would provide the best project outcome.



## 7.0 Phase V –Design Concepts (April - May 2013)

Phase V provided the opportunity for the stakeholders to refine their recommendations for the Wye Road business area, building upon the information and feedback gathered from the prior phases. Three potential design concepts were displayed at two engagement sessions for review and comment by the stakeholders. A summary of the prior engagement sessions was available for attendees. Stakeholders were advised of the engagement sessions via handouts that were delivered door-to-door (6<sup>th</sup> site visit) and emails. The large displays provided an opportunity for stakeholders to clearly see the concepts holistically as well as their individual components. A copy of the information delivered in included in Appendix V.

### 7.1 Phase V Objectives

Phase V combined the tools preferred by the stakeholders in Phase IV into three potential design concepts. The Phase V engagement sessions were held to gather feedback regarding potential stakeholder design concept preferences and the reasons for support or non-support.

### 7.2 Phase V Implementation

Two drop-in stakeholder sessions were held on Thursday, May 9 and Tuesday, May 14 from 3 pm – 7 pm at Sherwood Park United Church. Stakeholders received invitations and an 11x17 graphic of each of the potential design concepts via email or at their place of business (6<sup>th</sup> site visit). Stantec wanted to ensure all stakeholders were notified of the upcoming meetings and had the latest project information.

Three potential design concepts were developed using stakeholders' feedback from previous phases. The amount of modification required within the study area to implement the concepts ranged from slight (Concept 1) to substantial (Concept 3). The concepts were reviewed by a professional transportation engineer for viability prior to presentation at the session.

It was apparent that three stakeholders' properties would be particularly impacted by a few specific tools included in the design concepts. Stantec met individually with Ken Rice (Canadian Tire), Daniel Kuefler (Expert Lock Services), and Mitch Lewinsky (Park Mazda) before the stakeholder meetings. The business owners were not completely opposed to these tools and there inclusion in Phase V design concepts for further consideration.

Large display boards exhibited the potential design concepts generated from the various tools. These tools were explained in blue boxes, while the potential outcomes (positive or negative) were explained in red boxes. This made it easy for stakeholders to understand how the various tools worked together as trade-offs and outcomes were displayed for each concept. The display boards had a space for participants to comment using post-it notes. Participants were given two happy face stickers to place on the concepts that were their first and second preferences.

This method provided a certain amount of anonymity. The level of support and comments tendered were then compiled for use in Phase VI.

### 7.3 Phase V Results

Twenty-one participants representing fourteen businesses/properties attended these meetings. Seven stakeholders participated for the first time in this phase.

The process clearly indicated that Concept 2 was the preferred concept. Concept 3 received some support. There was very little support for Concept 1. Comments indicated that one of the tools in Concept 3 (modifying the existing signalized intersection at Safeway) was favoured by stakeholders and should be included in Concept 2.

All the input was summarized, noting the comments and level of support for each of the three potential design concepts. The complete summary is included in Appendix V.

Some specific stakeholder comments are outlined below:

#### **Concepts 1 and 2**

- Participants felt that overall safety would be increased if the slip-ramp into London Drugs off of Sherwood Drive was closed. They also supported the closure of the access to Petro-Canada station on Wye Road (westbound). It is important to note that the suggested closures were strongly opposed by the owners of the affected properties.

#### **Concept 2**

- Participants felt that the extension of Ash Street west to Ordze Road was a logical outcome of future growth.

#### **Concepts 2 and 3**

- Participants felt that adding a second westbound left turn at Sherwood Drive would benefit businesses south of Wye Road. They also supported widening Ordze Road and Sherwood Drive at Wye Road. Stakeholders felt that this would reduce traffic queuing as well as improve business accessibility and the overall safety of the road network.

#### **Concept 3**

- Stakeholders suggested modifying the existing signalized, limited access intersection located at Canadian Tire and Safeway to Concept 2.

#### **All concepts**

- Modifying the all-directional Sherwood Drive/ Ordze Avenue intersection to a right-in entrance/right-out exit was controversial. Stakeholders understood that trade-offs would be needed to create the two left turn lanes from Wye Road (westbound). However, there was strong opposition to the removal of the left turn onto Ordze Avenue (from northbound on Sherwood Drive) and the removal of the straight through movement (from Wal-Mart Site to Ordze Avenue).

### 7.3 Phase V Contribution to the Project

Stakeholders' review of the three design concepts provided the necessary input to refine and prioritize the concepts and their respective components (tools) into the final stakeholder recommended plan. Several new stakeholders were engaged and provided their input. All stakeholders received project information and were up-to-date with the progress of the project as the final phase of engagement approached.



## 8.0 Phase VI – Final Recommendation (June - July 2013)

Phase VI presented the final recommended plan as generated by the stakeholders for comment at two stakeholder meetings. Invitations and a summary of the process thus far were distributed door-to-door and through email.

### 8.1 Phase VI Objectives

The objective of Phase VI was to present the final concept to stakeholders and ask for their reasons for either supporting or not supporting the concept.

### 8.2 Phase VI Implementation

The two drop-in stakeholder sessions were held on Wednesday, June 12 and the Tuesday, June 25, from 3 pm – 7 pm at Sherwood Park United Church. Invitations and a graphic of the recommended concept, were hand delivered (7<sup>th</sup> site visit) and emailed.

The potential stakeholder design concepts from Phase V were developed into a one viable concept for review. The large display provided an opportunity for stakeholders to clearly see the recommended individual tools as well as the concept in its entirety. These tools were described in blue boxes, while the potential outcomes (positive or negative) were described in red boxes.

Each display board included space for participants to indicate their support for each concept and to write their comments. This providing an amount of anonymity and as the open house progressed, the ‘discussion-wall’ of comments made it easy for everyone to read. The level of support and all comments provided were compiled for a phase summary.

### 8.3 Phase VI Results

Sixteen participants representing fifteen businesses/properties attended these meetings. Three stakeholders participated for the first time in this phase.

The majority of responses were positive and supportive. Some comments are noted below:

- “Some improvement however it is NOT perfect.”
- “Looks good guys!”
- “Looks much better. Thank you for considering our feedback.”

All the input was compiled into a summary which can be found in Appendix VI.

Some specific highlights:

- The closure of the slip-ramp into London Drugs on Sherwood Drive and the closure of the access to the Petro-Canada station on Wye Road (westbound) continued to be controversial. As such, the closures were removed and the final stakeholder recommendation was for further study.
- Modifying the Sherwood Drive/Ordze Avenue intersection and retaining a left turn lane from Sherwood Drive (northbound) was supported by the majority of the stakeholders. However, the property owner of the ATB/Payless site expressed strong opposition to any modification and would prefer the intersection to remain an all-directional. In their view, this intersection is the most convenient of the three accesses to the property.
- There was strong support for improved access to Park Mazda, Smilie's Restaurant, Midas, etc. from Wye Road by building a new connecting roadway between Wye Road and Fir Street. It should be noted that this could only occur if and when it is initiated by the respective property owners.

#### 8.4 Phase VI Contribution to the Project

Phase VI was the culmination of the project. Potential design concepts from Phase V were developed into a one viable concept for review. The large display provided an opportunity for stakeholders to clearly see the recommended individual tools as well as the concept in its entirety. Participants provided their comments.

The stakeholders' comments and responses are summarized and included in the final Stakeholder Engagement Summary report.

## 9.0 Summary of Stakeholder Participation

Using an extensive outreach process, the Stantec engagement team personally visited every place of business (approximately 150) within the project area a minimum of seven times between September 2012 and July 2013. An additional twenty-one stakeholders who could not be contacted through the door-to-door visits were phoned and/or emailed and given information regarding the project and how to contact Stantec staff.

In addition to the conversations with stakeholders during the door-to-door visits, eighty-seven business/property owners provided responses to Stantec via email, telephone or mail. Ninety-one participants, representing sixty-seven businesses attended or contributed to the engagement sessions, many of whom participated in more than one phase.

Excluding the door-to-door visits, the Stantec engagement team was able to reach approximately 60% of the business and property owners.

Considerable effort was spent contacting as many stakeholders as possible as well as providing multiple opportunities over several months for them to offer their input. Despite this hard work, some stakeholders did not participate in the process.



## 10.0 Stakeholder Recommendations

The stakeholders developed their recommendations with three considerations in mind:

- Increasing road safety in the project area.
- Accommodating current traffic volumes and future increases.
- Enhancing short and long term business viability.

The following is a summary of location specific stakeholder recommendations, in no particular order.

### Wye Road: (in no particular order)

10.1.1	Add a second left turn lane for westbound to southbound traffic at Sherwood Drive.
10.1.2	Modify the signalized intersection between Ordze Road and Sherwood Drive at the existing Safeway access permitting left turns for westbound traffic south into the Canadian Tire site.
10.1.3	Modify alignment of Ordze Avenue to accommodate the proposed signalized access off Wye Road (refer to 10.1.2).
10.1.4	Add a dedicated right-turn/deceleration lane on Wye Road eastbound to Sherwood Drive southbound.
10.1.5	Add a right-out exit from the Save-On-Foods site westbound onto Wye Road.
10.1.6	Maintain the status quo for the current Petro-Canada access in the Save-On-Foods site from Wye Road.
10.1.7	Landscape the center median on Wye Road wherever appropriate.

#### Sherwood Drive: (in no particular order)

10.2.1	Add a second left turn for southbound to eastbound traffic at Wye Road.
10.2.2	Modify the uncontrolled access between Shoppers Drug Mart and Petro-Canada onto Sherwood Drive to become right-in entrance/right-out exit only.
10.2.3	Modify the Ordze Avenue/Sherwood Drive intersection (Wal-Mart access) to a right-in entrance/right-out exit, but still allowing for northbound left turn access to Earls, Home Hardware, the Canadian Tire site, etc.
10.2.4	Signalize the all-directional intersection at Sherwood Drive and Ash Street.
10.2.5	Maintain the status quo for the slip-ramp entrance to London Drugs.

#### Ash Street: (in no particular order)

10.3.1	Extend Ash Street west from Sherwood Drive/Range Road 233 to connect with Ordze Road/Crescent.
10.3.2	Connect this proposed Ash Street western extension to Ordze Avenue (refer to 10.3.1).

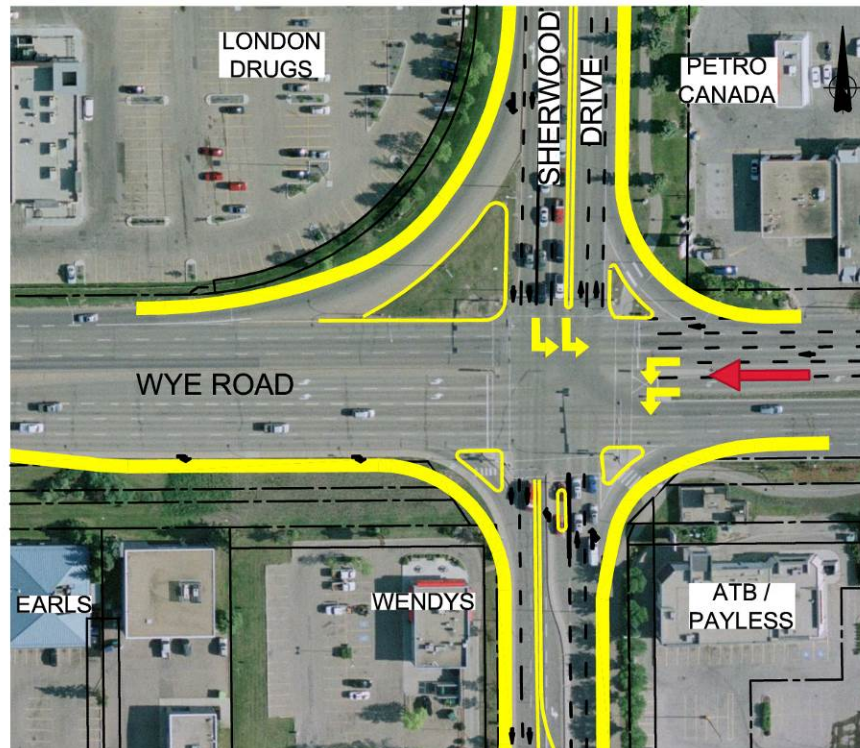
#### Ordze Road:

10.4.1	Widen Ordze Road.
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These stakeholder recommendations were developed after a very thorough process of stakeholder discussion and communication. Each recommendation is followed by discussion points. While consensus was not reached on every issue or proposed tool, the Stantec engagement team believes that these recommendations strike a balance between the various options and concepts favoured by stakeholders. The recommendations have yet to be fully verified from a technical or engineering standpoint.

## 10.1 Specific Stakeholder Recommendations for Wye Road

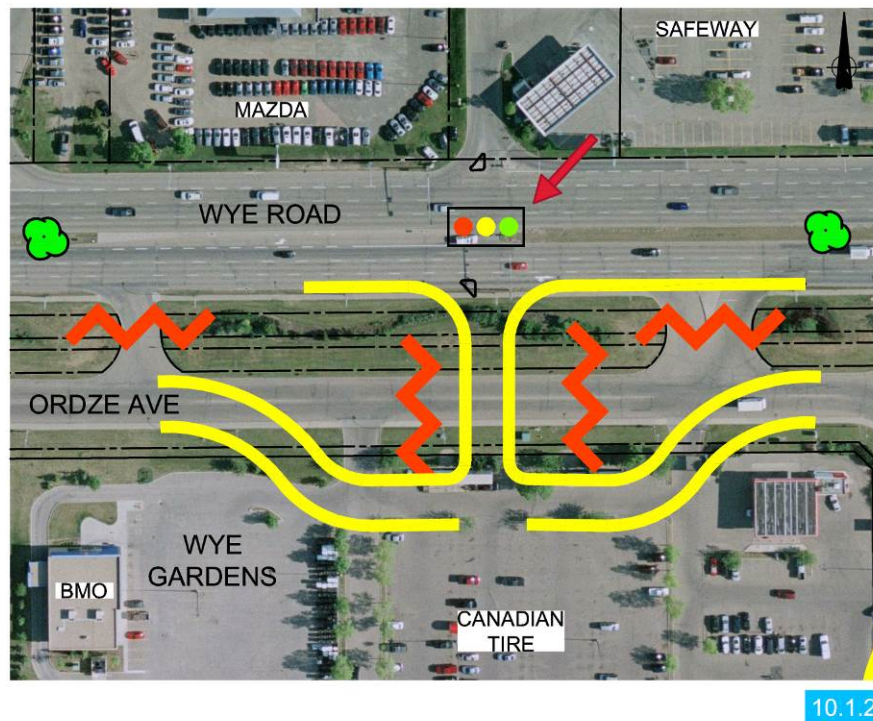
### 10.1.1 Add second left turn lane for westbound to southbound traffic at Sherwood Drive.



10.1.1

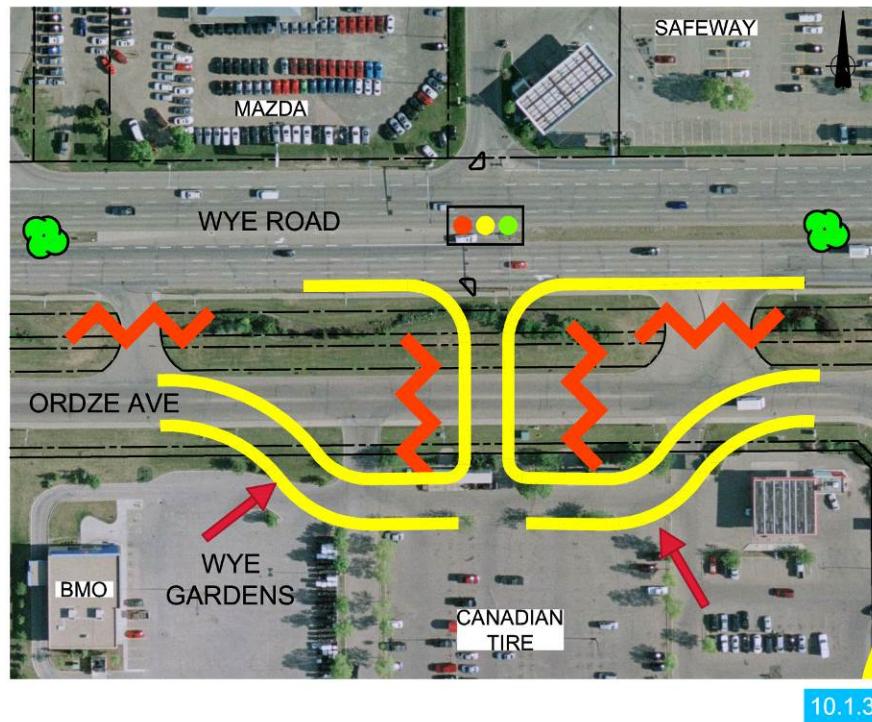
- a. The additional turning lane will allow more vehicles (customers) to access businesses in the area.
- b. Additional road widening will not be required as the pavement width is already available.
- c. This option is not viable without implementing 10.2.3.

10.1.2 Modify the signalized intersection between Ordze Road and Sherwood Drive at the existing Safeway access permitting left turns for westbound traffic south into the Canadian Tire site.



- a. This provides an additional access to the Canadian Tire site from Wye Road.
- b. This provides direct access and increases visibility to adjacent businesses.
- c. Straight through traffic from the Canadian Tire site to the Safeway site would not be permitted.
- d. Left turn access would be restricted at certain times of the day (as it is now).
- e. The two existing entrances and one exit on the south side of Wye Road must be closed.
- f. This would reduce possible locations for rear end collisions for east bound traffic from two to one between Ordze Road and Sherwood Drive.
- g. Modify the current alignment of Ordze Avenue to accommodate the proposed signalized intersection at the Safeway access (refer to 10.1.2).

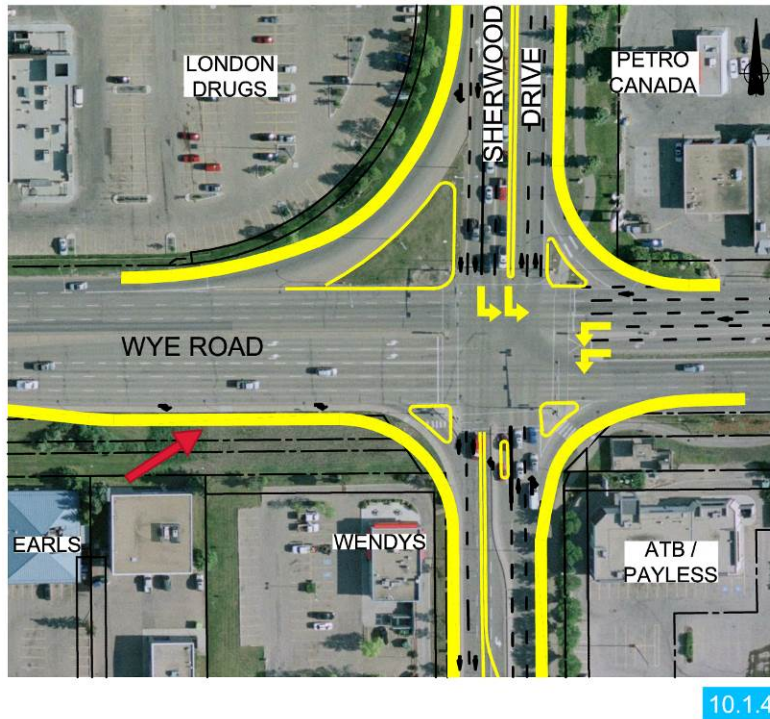
10.1.3 Modify the current alignment of Ordze Avenue to accommodate the proposed intersection at the Safeway access (refer to 10.1.2).



- a. This option provides an additional access to the Canadian Tire site from Wye Road which could lead to increased traffic volumes to the site.
- b. The Canadian Tire parking lot would be reduced in size. There is the potential to mitigate this loss by swapping land with the storm water retention pond immediately west of the Canadian Tire store. The retention pond would need to be relocated.



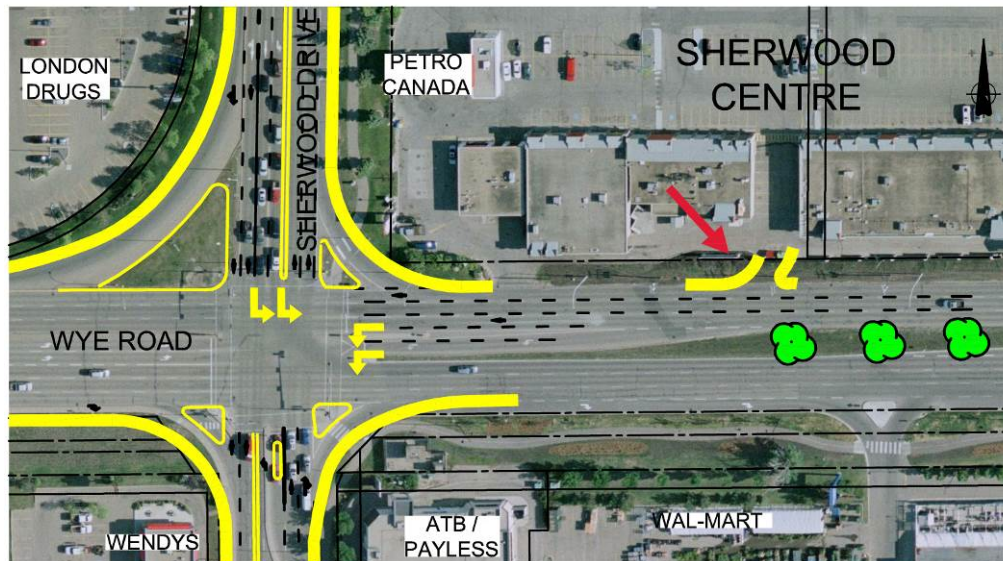
10.1.4 Add a dedicated right-turn/deceleration lane on Wye Road eastbound to Sherwood Drive southbound.



- a. An additional lane for turning traffic will accommodate more vehicles accessing businesses in the area.
- b. Safety will improve by accommodating vehicle deceleration outside of the through lanes.
- c. Traffic flows on Wye Road eastbound will improve by accommodating vehicle deceleration outside of the through lanes.
- d. Additional land will be required from adjacent property (Wendy's) to accommodate construction of the additional lane.



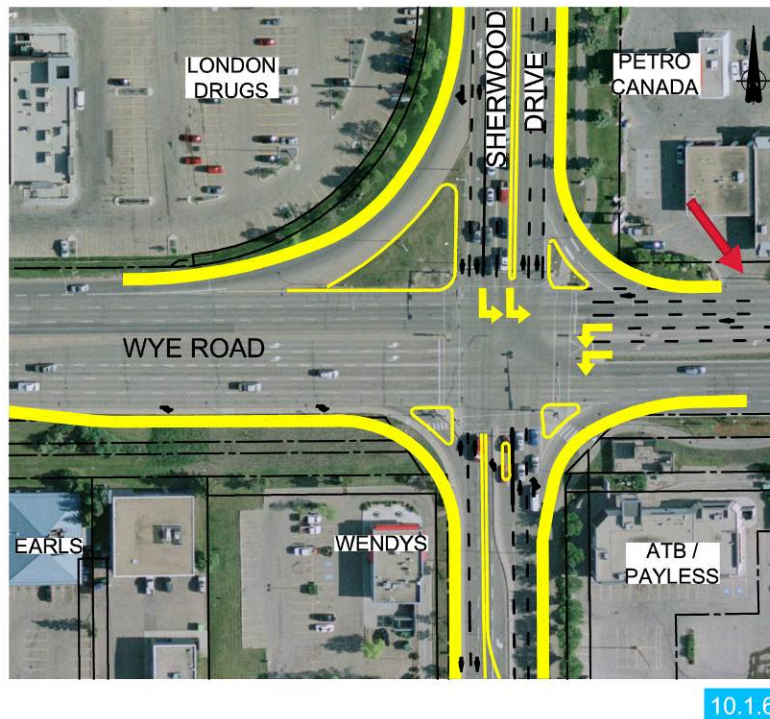
10.1.5 Add a right-out exit from the Save-On-Foods site westbound onto Wye Road.



10.1.5

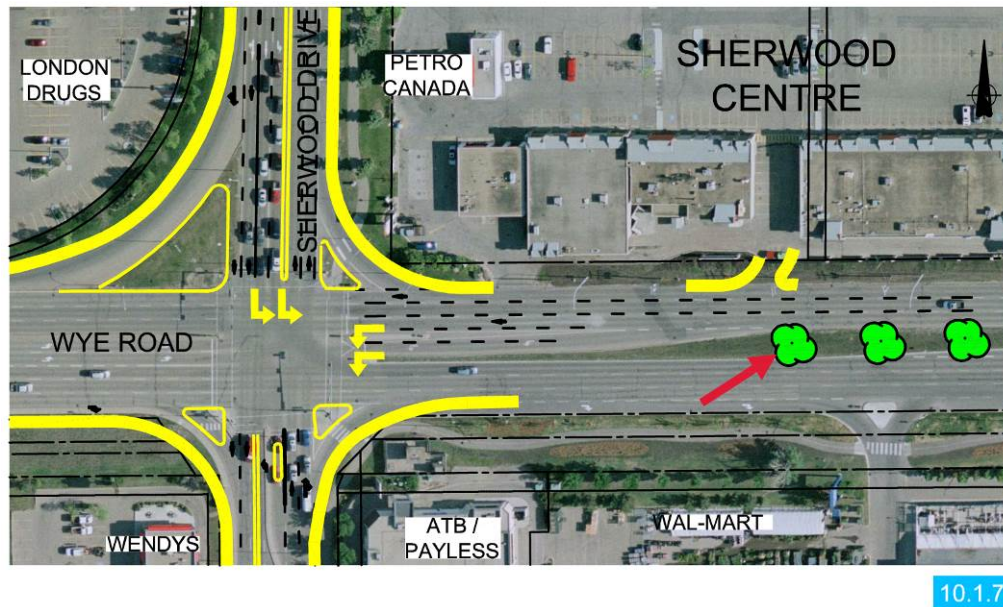
- a. This option provides an additional exit for the Save-On-Foods site.
- b. There is potential in the long-term to convert the access onto Wye Road to a right-in entrance/right-out exit when buildings (immediately north, east, and west) are eventually torn down and reconstructed.
- c. A right-in entrance/right-out exit was requested, but currently the room between the two buildings is insufficient to accommodate both movements. Additionally, stakeholders agreed that an exit was more important.

10.1.6 Maintain the status quo for the current Petro-Canada access in the Save-On-Foods site from Wye Road.



- a. Stakeholders determined that this access point is crucial for large trucks to access the adjacent businesses (i.e. fuel tanker trucks for Petro-Canada).
- b. There is the potential for this access to close in the long-term if the access point further east (10.1.5 noted above) is modified to right-in entrance/right-out exit and large truck access is provided at that location instead.

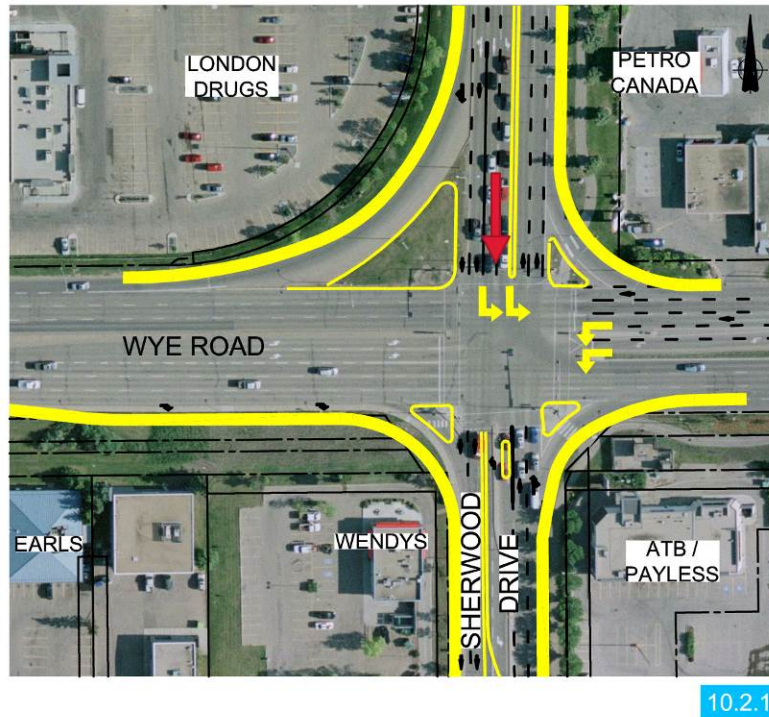
10.1.7 Landscape the center median of Wye Road wherever appropriate.



- a. Enhanced aesthetics of the corridor will reduce the perception of a 'sea of asphalt'.
- b. The addition of landscaping has been proven to reduce the speed along corridors similar to Wye Road. It is expected that the results would be the same on this roadway.
- c. The vegetation should be designed to maintain and accommodate the visibility of businesses along Wye Road and to allow for clear sight lines for motorists.

## 10.2 Specific Stakeholder Recommendations for Sherwood Drive:

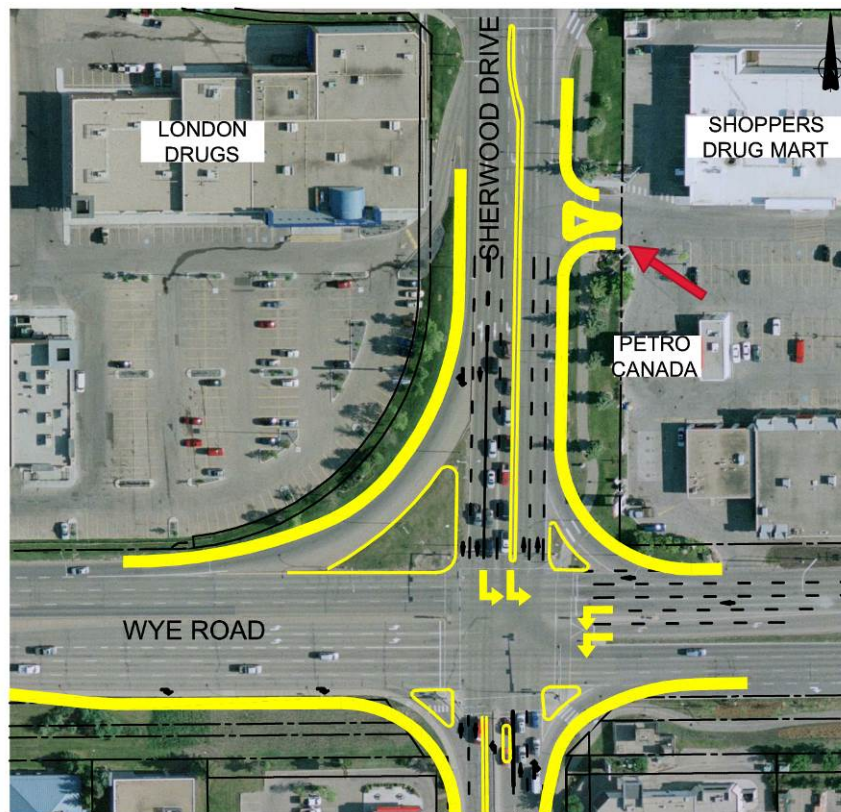
### 10.2.1 Add second left turn for southbound to eastbound traffic at Wye Road.



- The dedicated southbound to westbound right turn from Sherwood Drive to Wye Road would be modified to a shared straight through/right turn lane.
- An additional lane for turning traffic will allow more vehicles to access Wye Road eastbound.
- An additional lane for traffic left turning will reduce congestion on Sherwood Drive southbound.
- Reduced congestion will improve accessibility to businesses.
- The additional lane will require widening the intersection, which would entail acquiring land from adjacent properties (i.e. Wendy's).



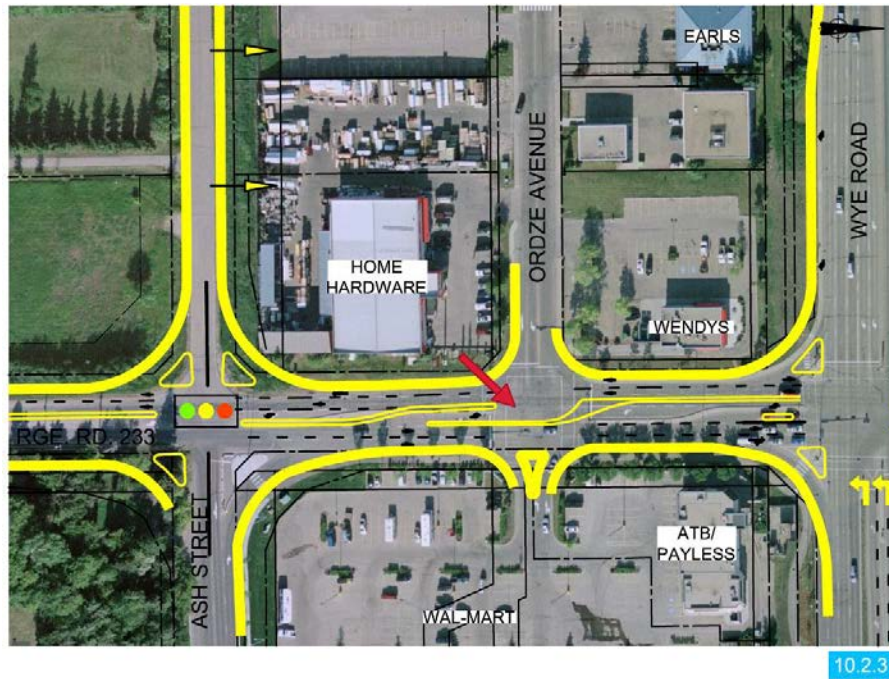
10.2.2 Modify the uncontrolled access between Shoppers Drug Mart and Petro-Canada onto Sherwood Drive to become a right-in entrance/right-out exit only.



10.2.2

- a. This option reduces the potential for collisions, particularly for left hand turns from the Save-On-Foods site to Sherwood Drive southbound.
- b. The access to the Save-On-Foods site for northbound traffic on Sherwood Drive is maintained.
- c. Traffic exiting the Save-On-Foods site turning right onto Sherwood Drive would be unimpeded by vehicles attempting to turn left.
- d. This option improves queuing on Sherwood Drive (southbound) for both through traffic and turning traffic.
- e. Option 10.2.1 is not viable unless this intersection is modified to prevent left turns out of the Save-On-Foods site.

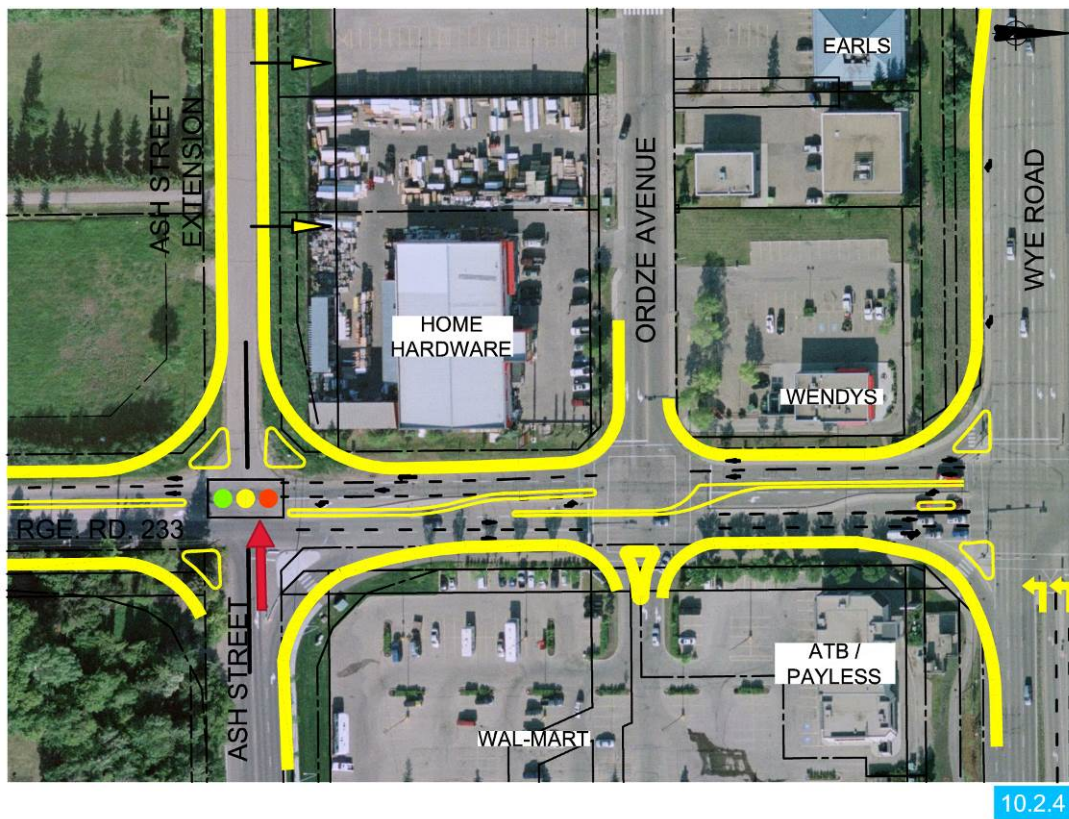
- 10.2.3 Modify the Ordze Avenue/Sherwood Drive intersection (Wal-Mart access) to a right-in entrance/right-out exit, but still allowing for northbound left turn access to Earls, Home Hardware, Canadian Tire site, etc.



- a. More left turning vehicles will be accommodated at Wye Road because the northbound to westbound left turn lane length is increased by two times.
- b. Traffic congestion for through traffic northbound and right turning traffic eastbound is reduced.
- c. The potential for collisions is reduced, especially for left hand turns into the Wal-Mart site.
- d. The access to Wye Gardens and Ordze Avenue for northbound traffic on Sherwood Drive is maintained.
- e. The three accesses to the Wal-Mart site are retained.
- f. The traffic exiting the Wal-Mart site turning right onto Sherwood Drive will be unimpeded by vehicles attempting to turn left go straight.
- g. Southbound traffic on Sherwood Drive would be unimpeded by vehicles attempting to turn left into the Wal-Mart site.
- h. Option 10.1.1 is not a viable option (additional left turn lane for westbound to southbound traffic at Sherwood Drive) unless this intersection is modified to prevent left turns into the Wal-Mart site.
- i. This kind of right-in entrance/right-out exit intersection is one commonly found in Sherwood Park and is the same as the 10.2.2 option.
- j. The property owner of the building housing Payless Shoes and the Alberta Treasury Branch is opposed to this modification and would prefer that the all-directional intersection remain.

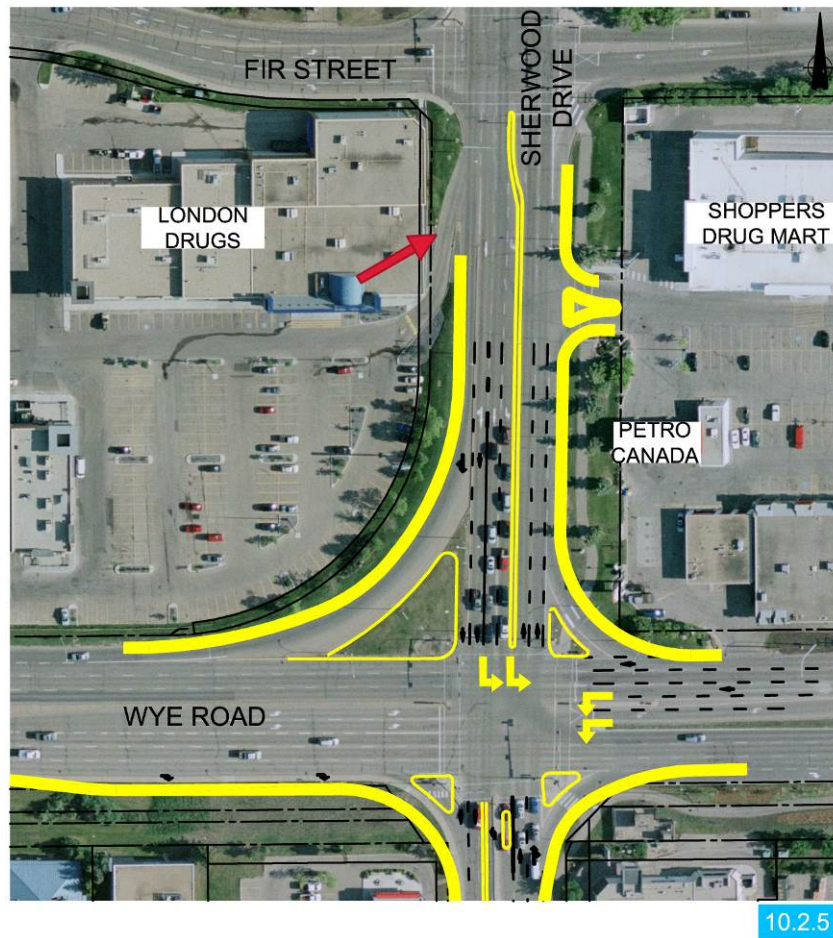


10.2.4 Signalize the all-directional intersection at Sherwood Drive and Ash Street.



- a. This will help manage traffic in the area especially if options 10.2.3 and 10.3.1 are implemented.

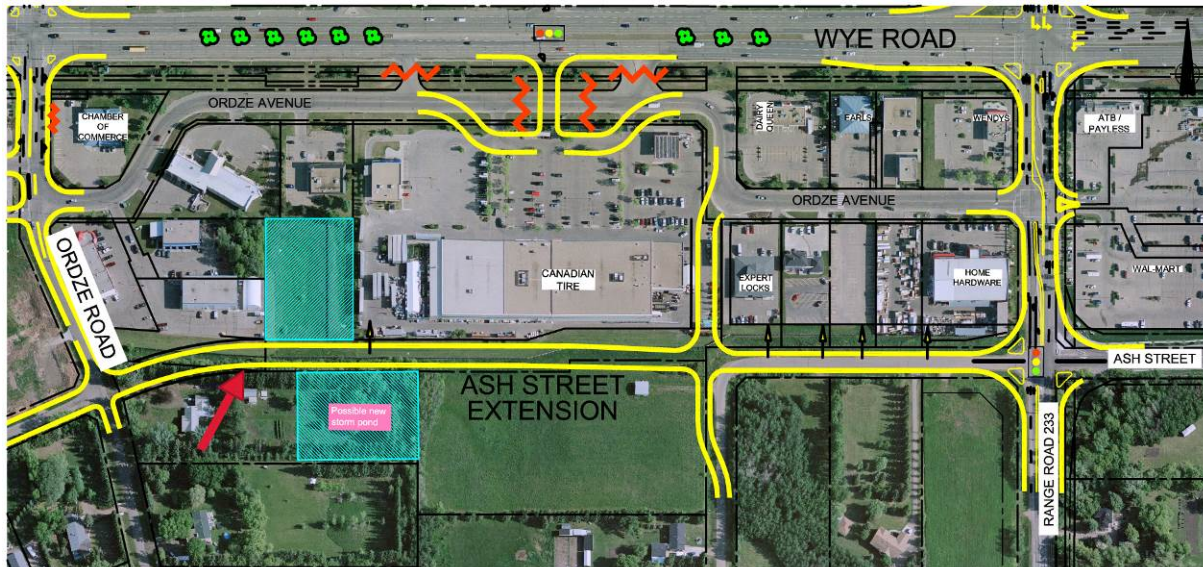
10.2.5 Maintain the status quo for the slip ramp entrance to London Drugs.



- a. Closing the slip ramp will reduce the possibility of rear end collisions.
- b. The property owner is opposed closing the slip ramp.

## 10.3 Specific Stakeholder Recommendations for Ash Street:

### 10.3.1 Extend Ash Street west to connect with Ordze Road.

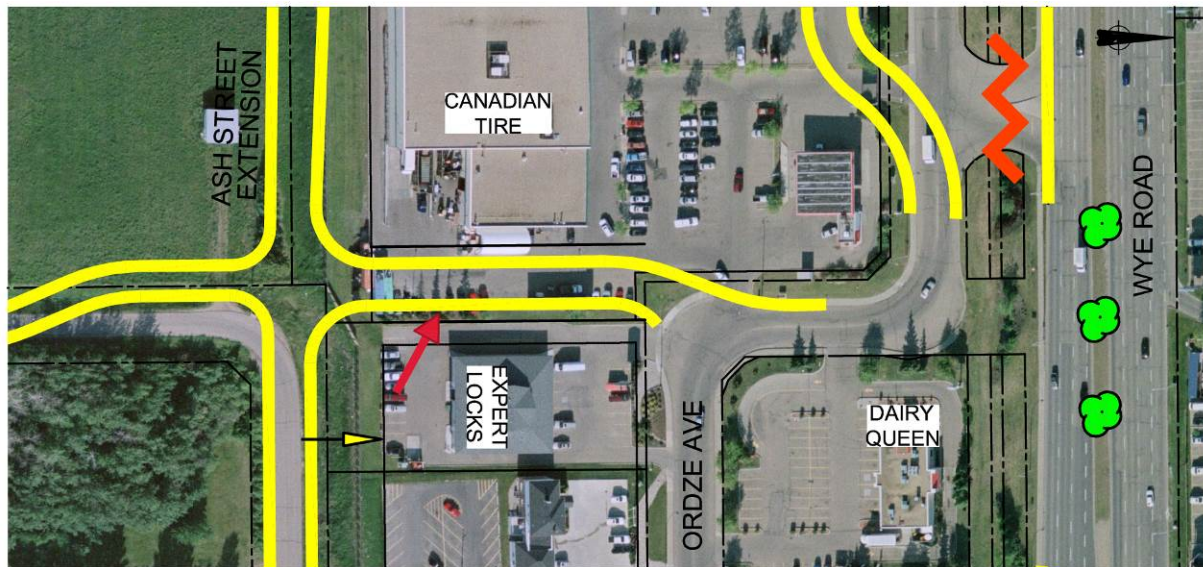


10.3.1

- This option provides alternative access to Ordze Road.
- The extension of Ash Street would necessitate removing the berm that exists along the adjacent northern properties. A similar/replacement barrier would have to be constructed on the south side of the road.
- The properties immediately north of the proposed Ash Street extension could repurpose their respective berm areas to create additional parking and/or storage if they wished.
- Driveway access could be provided from the proposed Ash Street extension to the properties (Home Hardware, Earls', Expert Locks).
- Additional land would be required from the private residential property at the Ordze Road/Ash Street intersection.
- This roadway connection is seen as a natural evolution of development in the area.



### 10.3.2 Connect the proposed Ash Street extension to Ordze Avenue.

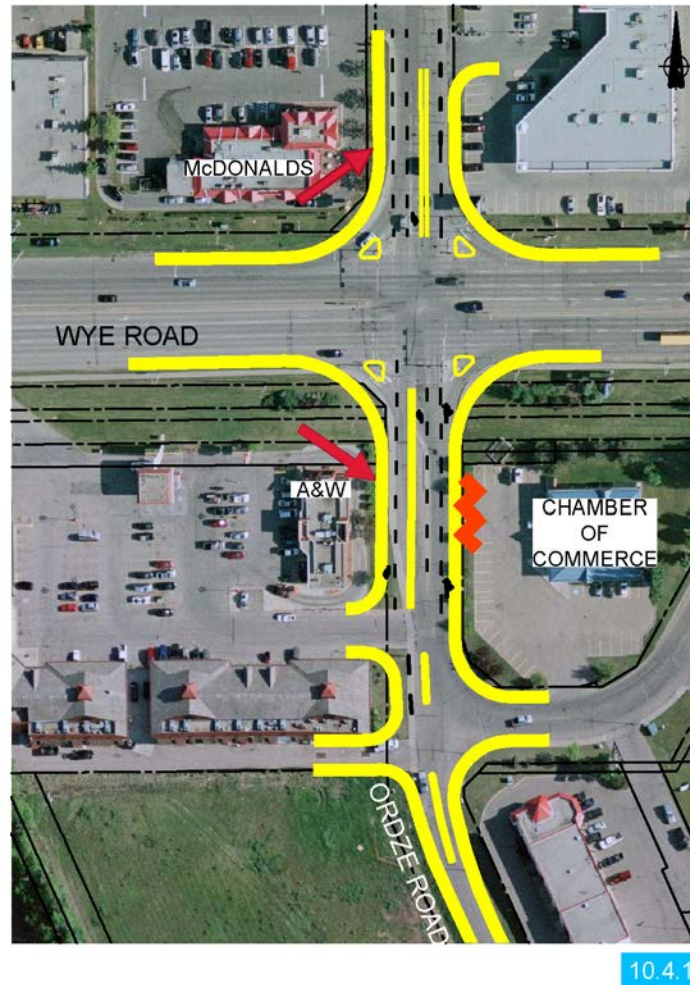


10.3.2

- a. Provides alternative access to Ordze Avenue.
- b. This would require land from the property owner (Canadian Tire). The local Canadian Tire business owner is not opposed; however, further discussions with Canadian Tire (locally and corporately) would be necessary.
- c. The land area requirements of this option along with those needed for option for 10.1.2. are roughly equivalent to the area of the current storm water management facility west of Canadian Tire. If the existing storm water management facility was relocated, it is possible that this land could be made available to Canadian Tire to compensate for the land lost due to the roadway modifications.

## 10.4 Specific Stakeholder Recommendations for Ordze Road

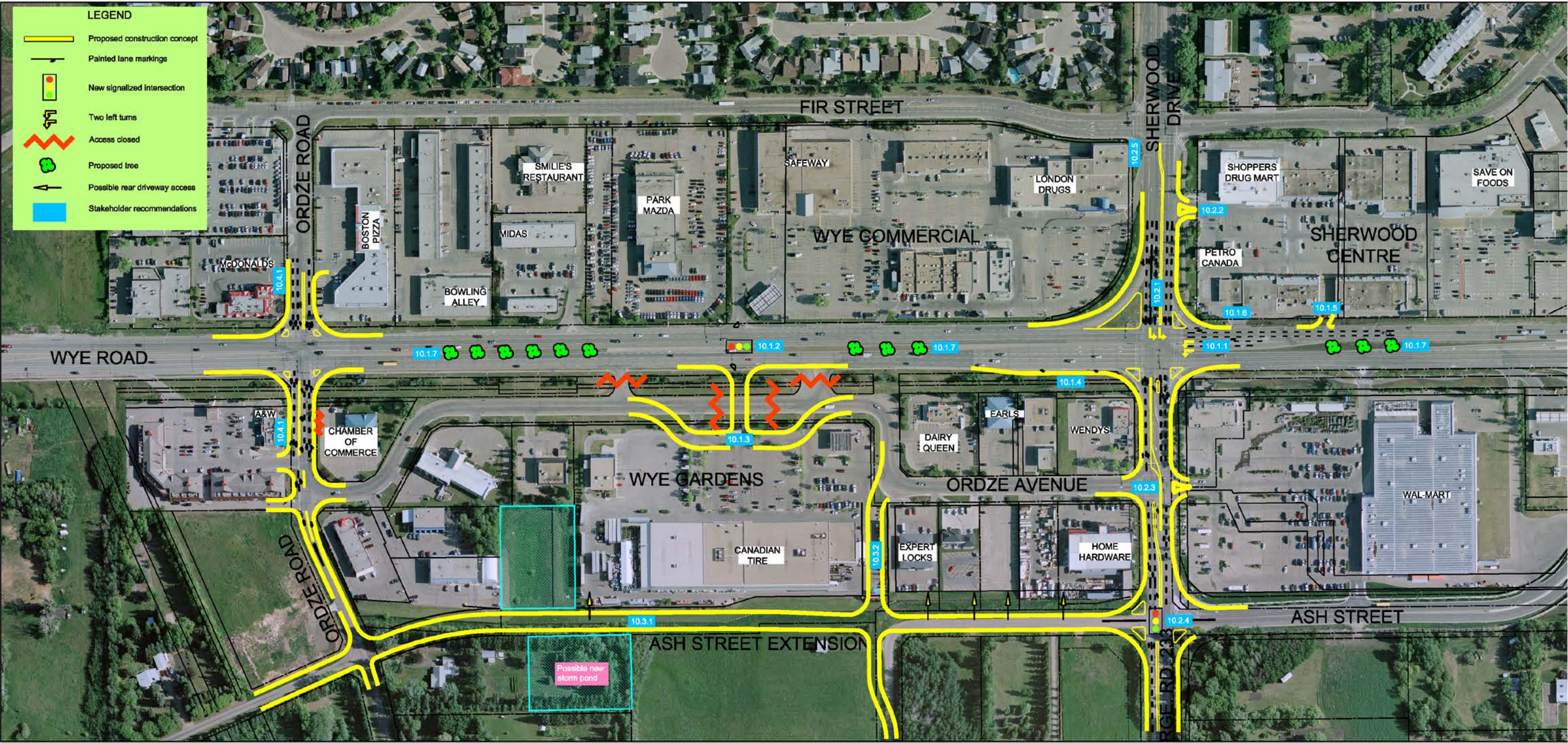
### 10.4.1 Widen Ordze Road.



- a. Accommodates more lanes for through traffic, therefore improving traffic flow.
- b. Increases the available length of the northbound to westbound left turn lane by three times.
- c. Reduces congestion for through traffic northbound and right turning traffic eastbound.
- d. Requires the closure of Chamber of Commerce access to Ordze Road. The Sherwood Park Chamber of Commerce is supportive of the closure as it results in additional parking spaces.
- e. Requires additional land from adjacent properties (i.e. McDonald's, A&W complex) to accommodate the widening.



10.5 Final Stakeholder Recommendation Design Figure





# Appendices

## Appendix I – Project Kick Off

Project Introduction Letter

## Appendix II – Handout Packages and Questionnaire Distribution

Handout Package and Questionnaire

Questionnaire Response Summary

## Appendix III – Face-to-Face Meetings

Meeting Information Package and Invite

Meetings Summaries

## Appendix IV – Stakeholder Feedback Sessions

Session Information Package and Invites

Session Display Materials

Sessions Summary

## Appendix V – Design Concepts

Session Information Package and Invite

Sessions Summary

## Appendix VI – Final Recommendation

Session Information Package and Invite

Sessions Summary

## Appendix VII – Master Stakeholder Engagement Database

Appendix I

# Project Kick Off

September 13, 2012

**Attention: Business Owner**

**Re: Stakeholder Engagement for Wye Road Improvements**

In 2006, Strathcona County retained Al-Terra Engineering to complete a functional study for Wye Road from Highway 21 to Highway 216. The purpose of the study was to determine optimal corridor improvements that would satisfy long term traffic demand and maintain safe roadway operational characteristics. Options such as road widening, possible intersection improvements and operational changes and existing and proposed access locations were evaluated.

Many changes have occurred over the last few years. They include the development of the Wye Crossing and Salisbury Village commercial center, re-development of Sherwood Center and, as a result of the functional planning study, construction of Ash Street behind the Wal-Mart, and widening Wye Road between Sherwood Drive and Hawthorne Road.

One section of the Wye Road Functional Plan remains unfinished - the section from Highway 216 to Sherwood Drive. The intersection of Sherwood Drive and Wye Road is under considerable pressure as traffic volume has increased over the years. The intersection can no longer efficiently and safely accommodate the number of vehicles using it. It is necessary to determine solutions that will ensure it functions well now and in the future.

Strathcona County wants to gather input from businesses owners in the area about their perception of the traffic issues. Al-Terra has retained a team from Stantec to work with representatives from adjacent businesses interested in understanding the issues and the engineering constraints. Stantec will then facilitate the development of possible solutions with the business owners. These suggested solutions will be reviewed by Al-Terra to ensure they are feasible and safe from an engineering perspective.

Over the coming weeks, you can expect to hear from the Stantec consultation team: Shawn Bravender, Teneya Gwin, Jonathan Mackay, and Terry Koch. They will be going door to door in the introduce themselves, starting the week of September 10, 2012.

This is a great opportunity to provide direct feedback and have your say on how the Wye Road corridor will look in the future.

Al-Terra and Stantec are looking forward to this consultation process. If you have any questions or comments, please do not hesitate to contact me. I can be reached at: [dleithead@al-terra.com](mailto:dleithead@al-terra.com) or 780-440-4411. You may also contact Shawn directly at [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com) or 780-917-7321.

Respectfully,  
**Al-Terra Engineering Ltd.**



Dana Leithead, P.Eng.

October 9, 2012

**Attention: Business Owner**

**Re: Stakeholder Engagement for Wye Road Improvements**

Dear Sir/Madame:

Strathcona County has retained Al-Terra Engineering to complete a functional study for Wye Road. County records indicate that you own property at 100 Ordze Avenue, which is within the scope of the project. Enclosed is a project information letter explaining the project background and the opportunity to provide feedback through the stakeholder engagement process as lead by Stantec. If you have any questions, or require any further information, please contact the undersigned below.

Respectfully,  
**Al-Terra Engineering Ltd.**

Dana Leithead, P.Eng.

Appendix II

# Handout Packages and Questionnaire Distribution





**Stantec**

## **Have your say about the next improvements on the Wye Road corridor!**

In 2006, Strathcona County hired Al-Terra Engineering to conduct a functional planning study for Wye Road from Highway 21 to Highway 216 to address roadway congestion and safety issues due to growth in Sherwood Park and an increase in traffic volumes.

As a result of this functional planning study, significant changes have occurred including the development of the Wye Road Crossing and Salisbury Village Commercial Center, re-development of Sherwood Centre, construction of Ash Street behind the Wal-Mart, and widening of Wye Road between Sherwood Drive and Hawthorne Road.

The section of Wye Road from Highway 216 (Anthony Henday Drive) to Sherwood Drive was also part of this study and is yet to be completed. Traffic analyses shows that drivers at the intersection of Wye Road and Sherwood Drive experience considerable traffic delays and that safety is a serious concern at this corner.

Al-Terra has hired a team from Stantec to work with the local business owners to gather their valuable input about traffic issues in the area and develop possible solutions. The suggested solutions will then be reviewed and analyzed (by Al-Terra and Strathcona County) to ensure their feasibility and that they are the safest options.

The attached survey is the first opportunity for you to provide input into ongoing improvements to the Wye Road corridor.

In the coming weeks, you will also hear from the Stantec representative as they go door-to-door to discuss the project. The project provides various opportunities to be involved in the project:

- Phase I – **Project Awareness**, an introductory project letter (attached) was hand delivered to every business within the limits of the project.
- Phase II – Initial **Stakeholder Engagement** will include personal visit with as many business owners as possible. These visits will further describe the project objectives, timelines and the consultation plan. A brief stakeholder survey will also be included.
- Phase III – **Direct Engagement** will occur during November and will consist of informal stakeholder meetings held at local businesses.
- Phase IV – **Project Recommendations**, a second round of stakeholder engagement meetings will be held to discuss topics and questions provided from previous input. With the input a mapping exercise will be conducted with techniques to choose the preferred solutions.
- Phase V – **Recommended Design and Consensus** will be sought. This phase will consist of a final series of meetings to receive stakeholder's final commentary on the recommended design.

Representatives from Stantec look forward to meeting with you during this consultation process. If you have any questions or comments, please do not hesitate to contact Shawn Bravender directly at [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com) or call at 780-917-7321.

**Please take time to complete the attached survey! Your input is appreciated.  
Questionnaires are due back by November 9<sup>th</sup>, 2012**

Regards,

**STANTEC CONSULTING LTD.**

**Shawn Bravender, B.Sc., RPP, MCIP, CET, IAP2-certified**

Associate, Urban Development - Planning

Tel: (780) 917-7321

Fax: (780) 917-7179

Shawn.Bravender@Stantec.com

# Wye Road Business Owners Questionnaire

Due back November 9, 2012

Wye Road corridor and business area from west of Ordze Road to east of Ash Street (see attached map) has experienced significant development and traffic growth over the past few years. That trend is likely to continue. We would like to hear from you about your experience and understanding on how Wye Road is currently operating. We also would like to know about your concerns with respect to future development growth and increase in traffic.

1. What are your overall concerns for the Wye Road business area on the attached map?

Please check on all that apply.

- |   |   |   |
|---|---|---|
| <input type="checkbox"/> Number of vehicles on the road | <input type="checkbox"/> Intersection design                | <input type="checkbox"/> Road-users' safety |
| <input type="checkbox"/> Vehicle access to businesses   | <input type="checkbox"/> Pedestrian/bicycle access          | <input type="checkbox"/> Transit use        |
| <input type="checkbox"/> Traffic signal timing          | <input type="checkbox"/> Eastbound / westbound traffic flow |   |

Others: \_\_\_\_\_  
\_\_\_\_\_

2. Please list your primary concerns with brief explanation.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

3. What do your customers and employees tell you about the traffic, access, safety, etc. to/from Wye Road?

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

4. What do you experience when you travel to other businesses on Wye Road?

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

5. Would you like to participate in Phase III – Direct Engagement? It will consist of informal stakeholder meetings held at local businesses in November 2012. Please provide you email address (above) so we can coordinate the optimum time for all interested parties.

- ☐ YES, I would like to meet and discuss with other business owners ☐ NO, thank you

6. Any additional comments?

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_





# WYE ROAD CORRIDOR AND BUSINESS AREA



October 21, 2012  
V:\1161\active\1161 101180\drawing\



Questionnaire Summary

Business Name & Address					1. Overall Concerns									1. Other	2. Primary Concerns	3. Customer/Employee Feedback	4. Personal Experience	5. Further participation?	6. Additional Comments
					# of Vehicles on Road	Intersection Design	Road-user's safety	Vehicle access to businesses	Ped/Bike Access	Transit Use	Traffic Signal Timing	E/W bound traffic	Other						
Interest in local business owner meetings - YES																			
1	Second Cup	113	120	Wye Road				X				X		Getting to second Cup when traveling eastbound is very difficult.	Access to our business (for those driving east-west) is poor, would love to see that improved.	When our Customers are traveling eastbound on Wye Rd.. They have to enter via Fir St. and pass through the Mazda lot. Many customers are uncomfortable with this and so they take their business elsewhere.	I have not comment-have not had any trouble	Yes	My understanding is that much of the lot Mazda uses is not actually their land-it is for future expansion. It would be good for us if this was made clear. Best solution for our business would be a left turn into our business area for eastbound drivers on Wye Rd.
2	Brentwood Animal Hospital		185	Ordze Avenue				X						Ordze Ave.	Previous "plans" negatively affected traffic onto Ordze Ave. by closing the Wal-Mart Intersection and redirecting traffic behind the berm in businesses, while annexing part of my property and Cdn Tire's. Net result=loss of income	No problem/complaints except when a 'gas war'(traffic from Cdn tire gas bas blocks road) and winter rain (slippery). No access signal timing complaints. Older clients appreciate signals at 233/Ordze Ave. Wal-Mart.	Some traffic delay ( I have to wait a few lights-not a big deal) at peak morning hours, otherwise I have little trouble getting anywhere I want to be in Sherwood Park.	Yes, pending 'times' I am the only person generating income for my location at this time.	I seriously question the wisdom of any construction on Wye Rd. at this time. The 216-Henday project is going to create major bottlenecks into the city along 216 accessing the park. Any problems on Wye Rd. may be resolved once that project is complete.
3	Park Mazda		983	Fir Street	X	X	X	X						Easement rd. between Park Mazda and strip mall linking Wye Rd. to Fir St.	Main concerns are 1.access to businesses from Wye Rd. Currently there are only access points form Fir St. 2. The easement rd. between Park Mazda and strip mall is extremely busy and is high risk for collision.	Often customers address concerns on accessing the dealership and the difficulties involved since there is no direct access from Wye Rd.	Personally I find the roads and traffic lights allowing access from other roads such as Sherwood Dr. take a longer than desired time to allow turning and accessing Wye Rd.	Yes	
4	Spark Heating & A/C		115	Ordze Avenue	X	X		X	X	X	X	X				Westbound turning southbound on Ordze Rd. very dangerous during non-peak hours when obstructed eastbound north turning transit buses.		Yes	
5	Earl's		194	Ordze Avenue				X			X				Change of access on 233 and Ordze. Change on Wye Rd. and Sherwood Dr. (left turns)	Not an issue at present.	No difficulty.	Yes	
6	Sherwood Bowl		975	Fir Street	X	X	X				X				Sherwood Dr. and Fir St.; eastbound left turn only allows 3-4 cars to get through intersection. Sherwood Dr. southbound at Wye rd.; traffic congestion for southbound and left eastbound traffic.	In general easy to exit but difficult to enter at times. Volumes increasing causing congestion.			
7	Aerotek Computers		918	Ash Street										Ash St. traffic is too low, better for businesses to have more traffic.	You indicated that lights at Wye and Sherwood Dr. would be changed to route more traffic down Ash. This would be very good all around	No concerns about safety.	No issues.	Yes	Really important to beautify the entrance into Sherwood Park just off the new Anthony Henday-need better landscaping features without messing up visibility to signage for stores.
8	Solid Skateboard Shop	49	301	Wye Road	X			X		X						Big delay turning east from Sherwood Dr., difficult turns into Wal-Mart on Sherwood Dr., light timing on Ordze through Wye through Fir, people turning south out of Sherwood Center are blocking traffic.		Yes	
9	Equestrian Supplies Unlimited		196A	Ordze Avenue							X				Severe traffic congestion at Ordze Ave. and RR233 during rush hour, impossible to turn from Ordze Ave. north because of volume and timing of lights at Wye and Sherwood Dr.	Never had a customer comment.	High volume of traffic, improperly timed traffic lights.	Yes	



Business Name & Address					1. Overall Concerns								1. Other	2. Primary Concerns	3. Customer/Employee Feedback	4. Personal Experience	5. Further participation?	6. Additional Comments
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10	Wild Wing	58	993	Fir Street			X	X				X			Somewhat difficult to access, have to go a long way but overall goes.	Good timing, but heavy traffic, this is always a concern in growing and developing cities.	Yes	New to the area, but interested in helping making decisions and learning more about development in the area.
11	Expert Lock Services Ltd. *see scanned copy		183	Ordze Avenue									No problems-could improve westbound traffic on Wye Rd. turning south on Z33 (2 lane like Baseline road.)	Al-Terra's 4 plans- already designed-decisions already made-county, engineering and Al-Terra just using this process to validate what they want to do.	No problems.	No problems.	Yes	The county on June 30, 2012 said that further intersection improvements are not in their 5 year construction plan and only when the intersection reaches capacity will activate improvements. The new Anthony Henday will remove traffic from Wye Rd.- so allow these to be completed first and then do a review.
12	CIBC	250	1020	Sherwood Drive	X	X		X				X		Roads do not seem to be adequate for the number of vehicles.	Too much congestion, very slow to navigate at times.		Yes	
Interest in local business owner meetings - NO																		
1	Silk Seam Tailors	114	937	Fir Street							X			We have received no feedback from customers	We have no issues at this time	No		
2	Marlin Travel		239	Wye Road		X		X				X		Traffic flow back up.	Traffic flow back up during peak times.	Long wait times at lights.	No	
3	Scotia bank	15	93	Fir Street		X	X	X			X		Access to businesses is already difficult. This will only get worse with widening and access restrictions.	Lack of access is the most often-heard complaint from my customers, especially for eastbound lanes.	Light timing is a lesser concern, but traffic volume is staggering.	No	I appreciate being given the opportunity to provide feedback via this survey. Thanks!	
4	Hart Transmission		110	Ordze Avenue		X	X	X			X		As a company off of Wye intersections and access to businesses are our main concern. We do not want it to be difficult for our customers to access us of they may go elsewhere	It is a very busy road at peak times and if an accident happens there is a gridlock.	Just heavy traffic.	No (sorry this is our busy time).		
5					X				X			X	Speed people travel for road conditions-then when there is an accident there is no way to go around.	Turning left into traffic always a concern-Wye and Sherwood Dr.-too many accidents.	Steady flow of traffic, most of the day.	No		
6	Chocolate Blue Hair	17	993	Fir Street		X		X			X		1. Intersection Wye Rd./Sherwood Dr.	Entrance to Wal-Mart, traffic lines up passes through intersection.	We need another lane going east/west to access into other businesses	No		
7	Mega Car Wash Ltd.		129	Ordze Avenue					X				My primary concern's about pedestrian and bicycle access.	I didn't hear any feedback.	Fine, I didn't see any problem.	No	Since our business is located off the Wye Rd. heavy traffic doesn't affect directly to our business. But occasionally some trucks park along the Service Road which disturbs customers' access to Mega Carwash.	
8	Insurance Discounts Ltd.	204	937	Fir Street										Nothing	Long wait at traffic lights.	No		
9	The Great Canadian Bagel		120	Wye Road		X		X				X	Eastbound traffic on Wye Rd. needs a turn off left into 120 Wye Rd. shopping center, it is too risky fro traffic to make an illegal U-turn to come back west on Wye Rd. to enter the shopping center.	They say it is difficult for just one entrance to the shopping center, you need one eastbound on Wye Rd.	As a business owner on 12 Wye Rd. if there is a turn off into this shopping center Eastbound then the traffic eastbound won't have to go to the intersection of Sherwood Dr. and Wye Rd. That will create less traffic in the intersection.	No		
10	Smile's Village Restaurant		981	Fir Street				X			X		This area is businesses only, must have easy access to their business, do not block any intersection during business hours.	Turning left on Sherwood Dr. to Fir St. accidents all the time.	Congested.	No.	Work at night was a perfect idea, no traffic, safer for the workers.	
11	Planet Beach	108	957	Fir Street	X									We are a not a customer-based office- no foot traffic.		No.		

Business Name & Address					1. Overall Concerns									1. Other	2. Primary Concerns	3. Customer/Employee Feedback	4. Personal Experience	5. Further participation?	6. Additional Comments
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12	Instant Imprints	101	977	Fir Street				X								Limited access to north side businesses in the Midas parking lot complex.	No.		
13	Expedia Cruise Ship Centers		967A	Ordze Road		X	X	X	X		X			There are no sidewalks on the north side of Wye Rd. 70km limit is to high for such a busy route (Whitemud is 80 km with no lights).	Too congested, speed too high for a busy corridor.	Difficulty getting in and out of the businesses.	No.		
14	BMO		145	Ordze Road	X	X	X	X	X	X	X	X		Safety.	Heavy traffic, congested, accidents/collisions.	Same as 2 and 3	No.		
15	Park Cleaners		963	Ordze Road		X		X			X			Wye/Ordze-intersection design to provide access to Ordze Rd. from eastbound lanes.	Nothing serious under present conditions. However, of the Wye/Ordze intersections changes, it could effect my customers' access.	I use Fir St. to the north and the service trade road south of Wye Rd.	No.	I would like to be updated on any changes to the traffic on Wye Rd. also on any flow changes.	
16	Brentwood Chiropractor Clinic	108	937	Fir Street	X	X					X			1 - Ordze Avenue & 233 into Wal-Mart is a disaster. Vehicles southbound going left back into Wye Road and block south access on 233. 2 - Wye Road & Sherwood Drive should have left turn light signal for northbound 233 traffic.	Why aren't lights synchronized?	1 - Excess traffic, lights not synchronized. 2 - Not on your list but access to 2nd Cup, Canadian Bagel off Fir Street through Mazda parking lot is a joke.	No		
Interest in local busiess owner meetings - UNKOWN																			
1	Wye Garden Dairy Queen		192	Ordze Avenue				X						I do not want any changes. It's already messed up they will just move the traffic two blocks with the same problems	Nothing.	No problems.	?	I do not want to lose any traffic accessing my business from Wye Rd. or Ordze Ave. I also cannot lose either of my entrances. Note: there are traffic problems at any rush hour at most main intersections.	
2	Lubex	41	99	Wye Road				X			X			My only concern as a business owner is the direct affect of construction on traffic. Construction is like cancer to a business. How will this project impact us?	Access to our business from Wye Rd. is not bad and not good. Westbound traffic waits at the light eastbound however can access easily. Getting back on Wye Rd. only problem is traffic back up from the mall.	Discomfort in the use of Wye Rd. unless I am traveling through the city. I use service routes for business.	?	Please don't disrupt traffic during construction, as ours is a volume based business	
3	Richard's Donair	31	99	Wye Road							X	X		I feel like a second degree citizen if I'm coming westbound to get to my shop or going eastbound on Wye because of the unfairness of the traffic signal timing.	Customers complain about how many accidents happen at the intersection.	Traffic signal timing is unfair.	?	We have experienced power shortage some hot summers. Will there be enough energy for all this expansion.	
4	Cali Nails	86	993	Fir Street													?		
5	Midas Auto Service		120	Wye Road		X		X						Boulevard down Center preventing turns, The boulevard could be a turning lane which would also eliminate congestion and intersections.	How hard it is to find our location, after finding it how hard it is to get into it. It would also help if we were placed properly on Google's maps GPS.		?		

Appendix III

# Face-to-Face Meetings

January 17, 2013

Dear Business and Property Owners

**Reference: Wye Road Functional Study – Stakeholder meetings**

As you may know, Stantec is working with business and property owners and managers to complete the last phase of the Wye Road Functional Study. So far we have heard back from over fifty businesses. Now it is time to start booking group meetings to hear different points of view and possible solutions. We need two hours of your time to hear your unique perspective.

Here are the details:

- January 29, 30, and 31
- Two hours sometime between 1 and 8 pm
- 8-12 participants
- Facilitated discussion

We are very flexible. We will do our best to accommodate your availability. If you are unable to attend those days, please contact us and we will see what we can do. Contact details noted below.

If your business has a meeting space that we could use, please let us know.

Yours truly,



**Shawn Bravender, B.Sc., RPP, MCIP, CET, IAP2-certified**

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# Wye Road Business Owners Stakeholder Engagement Agenda

## INTRODUCTIONS

### PROJECT INTRODUCTION and BACKGROUND

- Summary of background information provided on handouts previously delivered to each business.
- Review project steps leading up to now
- Why Wye Road? What are the concerns?

### THE ROADWAY IMPROVEMENT TOOLBOX

- Discuss a variety of 'Roadway Tools' which may be of consideration for this project.
  - i.e. Traffic signal phase timing, Road widening, Intersection treatments, access management, etc.
- What tools are missing that should be considered?

### HOW YOU CAN HELP!

- **Why** we want your input.
- What other opportunities are there for further input into the design of the Wye Road Functional Study.

### POTENTIAL SOLUTIONS

- Balancing the present and future needs of the portion of Wye Rd. as noted in the attached figure.
- What tools would best enhance growth now and in the future?

### NEXT STEPS

- Meeting summary and what to expect next





# WYE ROAD CORRIDOR AND BUSINESS AREA



October 21, 2012  
V:\1161\active\1161 101180\drawing\





The following is a compilation of the comments expressed during the series of local business owner meetings held from January 28 to February 5, 2013 and have been grouped by theme and/or intersection location (where applicable).

Below is the general meeting Agenda used.

### **Local Business Owner Meeting Agenda**

#### **INTRODUCTIONS**

#### **PROJECT INTRODUCTION and BACKGROUND**

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- Review project steps leading up to now.
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- What tools would best enhance growth now and in the future?

#### **NEXT STEPS**

- Meeting summary and what to expect next.

Every attempt has been made to ensure completeness and accuracy in capturing the thoughts expressed during the meetings. If further clarification is needed, please let Shawn know at

[Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com)

**Background comments:**

- The issues on Wye Road date back to 15 years ago when Wal-Mart was constructed.
- These issues were fought 6 years ago.
- There is an existing pipeline along Wye Road.
- Businesses paid the cost for sewer and water and the County didn't use it (along Ordze Avenue).
- Southberry is a new residential development. Three to four 18 story buildings are proposed.
- Possibly having southern access from Ash Street?
- Back road proposal (2008 ish) consisting of the extension of Ash Street west, and connecting to Ordze Avenue between Expert Locksmith and Canadian Tire, could remove loading access for certain businesses.
- When Wye Road was developed, there was no service road on the north side. ATCO pipeline is on the south side of Wye Road.

**Stakeholder Engagement:**

- The meeting summaries need to go to all business owners.
- When these issues were first raised, there were many discussions. Out of the discussions the possibility of a jug-handle was mentioned to alleviate turning and pressures.
- Can't plan an intersection just because it's engineered, need people's input.
- Dialogue with business owners is crucial.
- Original scope of project didn't take into account existing businesses.
  - People need trust in the process, that trust has been broken.
- The County has a copy of the numbers (future projection): what are the numbers from where this perceived problem is coming from. Need to see the numbers first.
  - Not just about now, also about planning for the next 20 years. What problems do we want to prevent, what lessons learned do we want to include?
- Invite RCMP to discuss traffic calming measures.
- Need effective communication with business owners during construction on Wye Road.

**Access:**

- One of the criteria to be used in determining access should be the potential to impact business.
- Business access is crucial for business viability.
- Improved methods are needed for traffic flow and access during accident clean-up time.
  - Accidents typically occur during peak traffic times as well as prime shopping times which limits access to businesses.
- If you improve access – you will improve flows.

### **Are there issues?**

- No issues, no problems.
- There are no traffic problems.
- Thought this issue had been put to bed. Every city has traffic issues.
- What's the project? What's the perceived problem? Why are we here?
- There were no traffic problems (until RR 233 roundabout was put in).
- Leave Wye Road as it is. I have never heard any complaints about Wye Road and I've been here since 1965.
- It would be easier to have a discussion if there was a plan to discuss. The original proposal (removal of left turns) was totally unacceptable. We want the status quo.
- What is the premise for these numbers? All of this is future forecasting, so why are we getting into this now?
- We're solving a problem that doesn't exist. Wye Road at peak hours is a through way. Why do we take away left hand turns?
- All of the left hand turns off of Wye Road are causing accidents.
- People comment that they prefer to stay on Baseline Road and that they never go to Wye Road due to the high traffic and poor accessibility.
- The Sherwood Drive and Wye Road intersection and the Sherwood Drive and Ordze Avenue intersection are causing the problems on Wye Road.
- Two lanes of traffic turning left and nowhere to go (onto Sherwood Drive).
- East bound traffic on Wye Road moves at a snail's pace after work.
- There should be no left hand turns on Wye Road unless there is advanced green arrows at all intersections on Wye Road.
- Intersection of Sherwood Drive and Wye Road "crazy".
- What's the magnitude of the issue? What's the capacity of the intersection? What's the goal, what the issue that you're trying to fix, traffic safety or volumes?
- Traffic congestion is huge from 7:30-8:30 AM. I avoid Wye Road to go home and take Ash Street instead.
- The business owner avoids the congestion on Wye Road and Ordze Road.
- Avoid Wye Road; take Ash Street and Ordze Avenue to get home.
- If there is an accident, that would eliminate access back to businesses and they would suffer.
- Volume of traffic has increased and this might be impacting business, because people aren't comfortable turning left and then crossing three lanes to access the businesses.
- The business owners at the meeting avoid Wye Road completely in the morning.
- Participant's wife was in an accident on Wye Road – know the dangers and problems on Wye Road.

### **Landscaping:**

- Landscaping to enhance businesses, not to visually block **(stated in multiple meetings)**.
  - Maintenance is important.
- Trees hide businesses and need to be addressed in size and numbers.
- We need to make Wye Road appealing. Plant trees down the length of the boulevard.
- We miss our trees.
- Removing the trees did improve business visibility.
- Trees might hinder the visibility of left hand turns.
- The flower bins that were installed, was that a traffic dissuasion measure?
  - Part of a community in bloom project.
- Trees would be good to slow traffic down.
- Would like to see landscaping in centre medians, i.e. trees.
- Planting trees within Wye Road median would reduce speed and would look appealing.
- Hopes that the County doesn't let trees and beautification get in the way of addressing the traffic issues.

### **Bicycle and Pedestrian:**

- Wye Road needs to improve pedestrian access and mobility **(stated in multiple meetings)**.
- More focus needed on driving traffic, not pedestrian traffic. This is a winter city and there's eight months of winter here.
- There is a need for sidewalks on Wye Road; sidewalks on the north side of Wye Road would not be safe.
- Need to extend walkway on Sherwood Drive, west side south of Wye Road.
- Walkability of Wye Road is not great. A sidewalk would be great. Not sure if that would mean that businesses would have to pay for the snow removal.
- There is a need for a sidewalk along Wye Road.
- Note: sidewalk running north and south on west side of RR233 is proposed.
- People want to come walking along Wye Road. Older people in the surrounding neighborhoods want to walk to businesses. Younger families moving in, the dynamic is changing.
- As a pedestrian, walking down Sherwood Drive to access businesses is unsafe.
- There is a need for pedestrian count down signals at all traffic lights.
- Pedestrian islands and improved access is needed along Wye Road.
- Fir Street is a pedestrian corridor and it will increase in time – need to provide and improve access for pedestrians.
- Pedestrian overpasses (bus depot has greater number of pedestrians).
  - Would like to see an elevated pedestrian crossing over Wye Road. It is not safe to cross Wye Road as a pedestrian.
  - Need more bike and pedestrian facilities (i.e. Elevated pedestrian crossing).
  - Would like to see a raised pedestrian crossing on Wye Road at the Ordze Road intersection. The length of the light is too short and unsafe for pedestrians to cross.



**Signalization:**

- Wye Road should have lane changes based on peak hours. For example the signaling on 97 Street near 118 Avenue.
- Where's the technology for signaling? Can we understand the traffic flows and adjust the traffic signals accordingly?
  - County decided to pick a third party contractor to monitor traffic. The video feeds are owned by this company so it makes it more complicated to access, but not impossible.
- Time left hand turn lights at Ordze Avenue and Sherwood Drive to align with other signaling.

**General Corridor:**

- Suggest having the traffic lanes widening east of project boundary.
- Is there room to add another lane on Wye Road?
- By adding "merge-ability" on Wye Road there is a chance that there is no need for an extra lane.
- AM peak are all westbound, PM peak are all eastbound.

**Drainage:**

- There's a storm water retention pond behind our property, I've never seen water in the pond (since 1987). There is an application into the city to rezone some of the property into C2 commercial, and apparently there's no longer the requirement for the land to be a storm water retention pond. If these changes occur, it affects the water run-off from the west end of the road.
- The storm water drainage pipes aren't the right size along Wye Road causing water backup and damage in my store. Piping for water evacuation has to be improved. There is standing water in the parking lot after every storm or snow melt causes.
  - There is only one drain in the parking lot (6 inch) so that the run off from the parking lot doesn't overload the storm water drainage. One storm will fill up the parking lot.
- Just make sure that storm water drainage considerations are factored into future expansions.
- There are issues with standing water in the Save-On Foods parking lot after a rain storm.

**Other Considerations:**

- Parking is an issue along the Chamber of Commerce building. Parking has become a problem since the Park and Ride for transit was removed.
- The noise attenuation barrier has removed access for trucks. Widen for truck access.
- Truck access, north side of Wye Road between Roadway and businesses – could it be widened?
- Turning into Rona is a challenge for semi-trailers (double).
- Rona signage is an issue (because Rona is so low).
- Visibility of Save-On Foods is an issue due to the grading.
- Add parking west and south of the Cafferty's mall site.
- Would like to have the concepts that are developed from the collaborations judged and reviewed by a traffic expert, to ensure that the concepts work well.

### General Comments:

- This project might remove business away from Wye Road. You can't talk about one piece without looking at the full picture. You need a holistic view.
- General Access:
  - It is not feasible to remove access to businesses.
  - Southern residents need access to Wye Road. This won't take away from businesses.
  - Good quality tenants need adequate access. If coming from the west, people have turn left on Ordze Road and then we have an agreement with Mazda to cut through their property. Left out or all directional turning at this point would be useful.
  - Tenants often complain about the need for better access to their businesses.
- Future growth in Sherwood Park will happen farther North.
- People are avoiding Wye Road; the Anthony Henday bypasses will remove even more traffic.
- There should be left hand turning arrows that mimic Baseline Road, this is a safety measure.
- The solution to signaling on Wye Road is to mimic Baseline Road.
- Left hand turns cannot be removed.
- Storm drainage along Ash street alignment: could it be accommodated elsewhere?
- The three intersections along Wye Road should align in timing.
- In the past, 1,100 hand-drawn plans have been submitted to the residents. I have drawn up a structural plan or concept where lights are removed by Home Hardware.
  - A visual presentation of concepts is needed for better a understanding.
- What are the chances of Ordze Road being widened?
- 16A at Spruce Grove doesn't seem to have the same problems and traffic issues.
- Grading challenges of site are noted.
- 8th Street in Saskatoon is a great example. If U turns were allowed (on a green light), it might be helpful.
  - Site distance important for U turns so that you can see vehicles coming.
- C2 rezoning: a two story L-shaped development is planned adjacent to our property.
- Weather and traffic congestion and ease of accessibility is a concern.
- Transit woes on west end of project site.
- The installation of red light cameras is the best thing done for Wye Road to reduce the speed.
- Red light camera at Ordze Road and Wye Road effective.
- Time induced speed limits.
- A possible solution may be to change the speed limits during peak traffic hours.
- In 20 years, density of site and adjacent land uses will increase.
- In 20 years, if buildings are not recycled – then the management firms are not doing their business right.
  - Opportunity to phase changes with development/change of use.
  - Potential recycling of buildings may start at the eastern end and work westwards.
- Likes the possibility of putting in solid curbs to direct turning traffic into the right lane (multiple applications).

## SHERWOOD DRIVE

- The channeling on Sherwood Drive, east of London Drugs is unsafe.
- Close the slip ramp into London Drugs.
- Retain access to London Drugs, however, open to improving it.

### Sherwood Drive @ Sherwood Centre:

- Intersection on Sherwood Drive to Sherwood Centre is too close to Sherwood Drive and Fir Street.
  - Multiple conflict points as left hand turns cross over to far lanes, combined with problematic queuing.
- Access to the Sherwood Centre complex, to CIBC, should be from Fir Street near the Save-On Foods. The alley way would have to be widened and the parking removed.
- **Consensus in all meetings** that the entrance to Sherwood Center needs to be revised.
  - People aren't using this intersection because they feel it's too dangerous.
  - Do not completely close the entrance.
  - Make access to Sherwood Centre a right-in / right-out.
  - Eliminate all left hand turns.
    - Northbound left hand turns – too short a distance to be safe and effective.
  - Use a center median to enforce no left hand turns.

## **WYE ROAD**

- Will Wye Road be widened?
- Split Wye Road and extend Ash Street. Take Wye Road from two-way, to one way and another one way on Ash Street?
  - One consideration is that this would affect businesses that depend on Fir Street access.
  - Ordze Avenue and Sherwood Drive: one-way on Ordze Avenue could eliminate a lot of problems.
- Provide for transition lanes (eastbound) on Wye Road to discourage turning onto Wye Road and immediately crossing the three lanes – between Ordze Road and Sherwood Drive.
- The concept of turning Wye Road into something like Gateway Blvd. and Calgary trail – having commuter traffic on another road would not be beneficial to businesses along Wye Road.

### **Wye Road @ Hawthorne Street:**

- No comments.

### **Wye Road @ Ash Street**

- Two left turning lanes going southbound on Ash Street.
- There is congestion turning left from Wye Road onto Ash Street. Potential accidents from the speed of the vehicles turning left onto Ash Street and the congestion on Ash Street.

### **Wye Road & Sherwood Centre Site:**

- Access to Sherwood Centre from Wye Road could be shifted east to the gap between the two northerly buildings.
  - Right-in / right-out access would work.
  - Remove current easterly access – or heavily modify for right-in / right-out and to prevent flows from crossing three lanes of Wye road to turn left onto Sherwood Drive.

### **Wye Road @ Sherwood Drive:**

- Need two turning lanes from Wye Road unto Sherwood Drive to avoid line-ups at Wal-Mart.
- Sherwood Drive and Wye Road intersection needs left hand turns in all directions.
- 50,000 vehicle trips per day coming into Sherwood Drive and Wye Road intersection.
- Why are we focusing on the 4<sup>th</sup> largest traffic location when there are larger priorities?
  - Planning for future is key; all intersections are being looked at, including this one.
- Westbound on Wye, turning left (south) on Sherwood Drive – should be two left hand turning lanes, signalized.
- At heavy traffic times, you can't turn on to Sherwood Drive. The three lanes are backed up from Fir Street. Timing of light is an issue.
- This wasn't a problem 30 years ago, there sure is now, does this intersection make sense in today's traffic on Sherwood Drive? I avoid this intersection (Sherwood Drive and Wye Road).

- Channelization curb is unsafe: people have to make a decision in the middle of an intersection. Sherwood Drive curb should be obvious and absolutely impassible. Sign gets plowed down on a regular basis.
- Intersection on Sherwood Drive (north of Wye Road) and businesses (Shoppers Drug and London Drugs) queues to a failure point.
  - Not safe.
  - Just as many accidents as Sherwood Drive and Wye Road.
  - Signage gets hit consistently.
  - Significant J-walking.
- Turn out from Canadian Tire (right) and then having to switch three lanes to turn left unto Sherwood Drive is very dangerous, (heading east bound).
- Turning left from Sherwood Drive: could there be just a left turning light like on Baseline Road? Left turning signals in all directions would be helpful.
- Intersection of Sherwood Drive and Wye Road - Left hand turns, in all directions, signalized.
- Westbound on Wye, turning left (south) on Sherwood Drive – should be two LH turning lanes, signalized.
- Stop the left hand turns from east to west on Sherwood Drive to utilize the jug-handle.

#### **Wye Road & Wye Commercial Site:**

- Wye Commercial; current right-out only – suggest adding a right-in.
  - Provision of a transition lane may be needed.

#### **Wye Road @ Second Cup / Mazda Access:**

- Mazda cross-access agreement: legal agreement with previous owner because access was supposed to be removed from title but wasn't. Integrated Management Realty maintains the agreement. Snow piles reduce access and make it unsafe.
- Please provide a better access into the Second Cup et al business site. i.e. a left hand turn.
- There are a lot of accidents coming into our business parking lot.
- Mazda, Second Cup, etc. can't be accessed from eastbound Wye Road.
- Everyone would benefit from an all directional straight across from Fusion Music.
- Second Cup has poor access from Wye Road. McDonalds has great access with plenty of space.
- The retaining wall on the north side of Wye Road near the Second Cup gives no visual to the businesses on the north side such as Second Cup; potential customers do not see the businesses there.
- The biggest issue to the businesses on the north side is the poor access from Wye Road. Customers are unaware that the businesses can be accessed through the Mazda dealership. Second Cup loses all customers traveling east.
- The entrance from the Mazda dealership is not ideal for Mazda or the commercial complex.
- The entrance from Mazda is very narrow and unsafe.
- There is a need for a left hand turn at the Mazda dealership on Wye Road.



- Parking at the park and ride is an issue. There are individuals that park in the commercial complex where Second Cup is located for the park and ride.
- Participant loves the idea of having a set of lights and a new road running north to south beside the Mazda dealership and serving access to Canadian Tire.
- Going west on Fir Street, there is no clear access to Wye Road, or to the businesses between Wye Road and Fir Street. A road west of the Mazda is needed.

#### **Wye Road @ Ordze Road:**

- Intersection of Ordze Road and Wye Road **(stated in multiple meetings)**:
  - Signal light timing could be improved.
  - Will queuing be a problem once the Transit park and ride pressure is resolved??
- Transit park and ride pressure :
  - Add a lane to right-in on Wye Road at Cafferty's (westbound).
- Ordze Road and Wye Road intersection: Widening intersection to put in a hard curb to ensure that people are staying in the right lane. Same thing could be done coming out from Wye Gardens and onto Wye Road and jumping lanes.
- Distance factor: Left turn from Ordze Road to Wye Road blind spot to see someone coming in on the left.
- Intersection at Ordze Road and Wye Road is the worst intersection within the project boundary.
  - Left hand turns are insane.
- There is always a traffic jam in the A&W parking lot at noon. Vehicles cannot get out of the parking lot causing major congestion. There is a great need for a new access out, it is impacting sales.
- The exit onto Wye Road from Ordze Road is too narrow. There should be more than two lanes in case of an accident to keep traffic moving.
- When there is an accident east of A&W on Ordze Road it prevents all access to the businesses and no exits.
- Would like to see a raised pedestrian crossing on Wye Road at the Ordze Road intersection. The length of the light is too short and unsafe for pedestrians to cross. (also noted in Bicycle & Pedestrian comments)
- Ordze Road, north of Wye Road is very accident prone.
  - Widen Ordze Road to accommodate two lanes in each direction.
- Move the right-in to Cafferty's further west.
- Create parking on the west side of Cafferty's.
- Right-in / right-out by Wal-Mart – NO set of lights!
- Ordze Road queuing space too small.

## **FIR STREET**

- Directional signage on Fir Street so people know where businesses are located.
- Can Fir Street be widened?
- Good access from Wye Road might alleviate traffic on Fir Street.
- Free flow lane (westbound) from Fir Street to Wye Road (west of commercial district).
- The alley on the west side of Save-On Foods from Fir Street is very dangerous. There is not enough room for two way traffic.
- Access from Fir Street to the west side of Save-On Foods is too dangerous to use.
  - Grade change is notable.

### **Fir Street @ Ash Street:**

- No comments.

### **Fir Street @ Sherwood Drive:**

- There is too much stop and go traffic on the west side of Wal-Mart until past the London Drugs.

### **Fir Street @ Ordze Road:**

- Intersection of Ordze Road and Fir Street:
  - Widen road way.
  - Fly over from Anthony Henday to Fir Street??

## ASH STREET

- Extend Ash Street easterly to Hawthorne Street alignment **(stated in multiple meetings)**.
- Extend Ash Street westerly to Ordze Road alignment **(stated in multiple meetings)**.
  - Connect to Canadian Tire on the west side, currently the storm water management facility site.
- Ash Street connection from west of Canadian Tire to Wye Road **(stated in multiple meetings)**:
  - Right-in/right-out.
  - Signalize.
  - Close right-in/right-out access (x2) from Ordze Avenue just north of Canadian Tire.
  - Intersection of proposed connection and Ordze Avenue problematic.
- Extend Ash Street connection from Canadian Tire to Fir Street, with signalization at Wye Road **(stated in multiple meetings)**.
  - Aligned with current Mazda / Second Cup access.
  - The Mazda access is perceived as a private access.
  - The grades (elevation change) may be a challenge.
- Connect Ash Street to Wye Road between Expert Locksmith and Canadian Tire **(stated in multiple meetings)**.
  - This would hurt Expert Locksmith, maybe even have to buy out Expert Locksmith.
- Utilize Ash Street as major secondary route, considering this as a long-term plan for the future.
- Left turn from Ash Street (northbound), meridian sticks too far into the intersection.
- In 15-20 years, properties behind Ash Street will likely become commercial, so you'll need Ash Street.
- Like the idea of extending Ash Street. Would like to see signals in their part of the street. Would be interested in selling part of property for a parking lot.
- Jug-handle: yes, people are using it, although not enough people seem to know it's there.
- Use Jug-handle as a One Way route back to Wye Road.
- Putting a jug-handle behind Canadian Tire wouldn't make much of the difference because of the road already existing in front on the Canadian Tire.
- The jug-handle should continue through to the west side of A&W to elevate the traffic on Wye Road.

### Ash Street @ Sherwood Drive:

- Double southbound lanes on Sherwood Drive (south of Wye Road).
- The alignment of the lanes on Sherwood Drive (at intersection with the jug-handle) are very confusing, the lane feels as if it is a turning lane and one way traffic.
- Intersection at Ash Street and Sherwood Drive – widen to two lanes each way.
  - Current lane alignment and signage confusing (north bound from Ash Street).
  - Transition difficult.
- Having a roundabout on Sherwood Drive and Ash Street maybe another option.

## ORDZE AVENUE

- Ordze Avenue on weekends, patrons coming out from Cafferty's are extremely dangerous.
- There is a curb on Ordze Avenue, east of A&W that causes accidents regularly. In the winter many vehicles don't see the curb and end up hitting the pole on the curb.

### Ordze Avenue @ Sherwood Drive:

- Lights at Ordze Avenue are too short (left turn).
- Accidents occur in the entrance to Wal-Mart and Sherwood Drive.
- Intersection Ordze Avenue & Sherwood Drive **(stated in multiple meetings)**:
  - Need lanes for left hand turns queuing, flow through, and right hand turns.
  - Modify Wal-Mart Access:
    - Right-in / right-out Wal-Mart access.
    - Impacts to ATB and Payless Shoes.
  - Should not be signalized.
  - Create additional access to ATB/Payless/Wal-Mart south on Ash Street.
- Close Ordze Avenue north of Home Hardware. Have access to these businesses from the back.
- Time left hand turn lights at Ordze Avenue and Sherwood Drive.
- The lights west of Wal-Mart on Sherwood Drive are too close together, it is unsafe and not to code.
- The queuing of lights between Wal-Mart and Wye Road is very poor.
- Remove the lights in front of Wal-Mart on Sherwood Drive.

### Ordze Avenue @ Ordze Road:

- Would like to see flashing lights on T-intersection of Ordze Road and Ordze Avenue.
- A round about near the Chamber of Commerce would be useful.
- 30 Km/h zone.
- Needs to be a flashing light near the Cafferty's' intersection.
- Major traffic congestion at confluence of Ordze Road and Ordze Avenue.

Appendix IV

# Stakeholder Feedback Sessions





**Stantec**

February 28, 2013

Dear Business and / or Property Owner,

**Reference: Wye Road Stakeholder Engagement**

As you may know, Stantec is working with managers, business owners, and property owners to complete the last phase of the Wye Road Functional Study. To date, many of you have provided your insights and perspectives.

We have scheduled two meetings to relay what we have heard so far. Additionally, we would like to discuss with you the various tools that you would like to be considered in the preparation of viable concepts. These concepts will be designed after we hear from you in this step. The meetings are a drop-in format so feel free to come for as long as you would like.

Here are the details:

- **Location: Sherwood Park & District Chamber of Commerce main boardroom (100 Ordze Avenue)**
- **Thursday, March 7 - drop in anytime between 3 PM – 7 PM, and**
- **Tuesday, March 12 - drop in anytime between 3 PM – 7 PM**
- **Light refreshments will be provided**

Thanks to the Sherwood Park & District Chamber of Commerce for providing us with their facility to host both meetings.

We look forward to hearing from you!

*\*Your email address will ONLY be used to transfer project information and updates as well as coordinate local meeting times.*

If you would like to be removed from this email list, please let us know at [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com)

Kind regards,

**Shawn Bravender, B.Sc., RPP, MCIP, CET, IAP2-certified**

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[Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com)



# Drop in to our second Open House tomorrow!

As you may know, Stantec is working with managers, business owners, and property owners to complete the last phase of the Wye Road Functional Study. Many of you have provided your insights and perspectives. We have scheduled two meetings to relay what we have heard so far. Building upon the success of our first meeting, we wanted to specially invite you to the meeting on Tuesday.

Additionally, we would like to discuss with you the various tools you would like to be considered in the preparation of viable concepts (to occur after we hear from you in this step).

The meetings are a drop-in format so feel free to come for as long as you would like.

All participants of both meetings will be entered into a draw for a ¼ page advertisement in the Sherwood Park news\*

Here are the details:

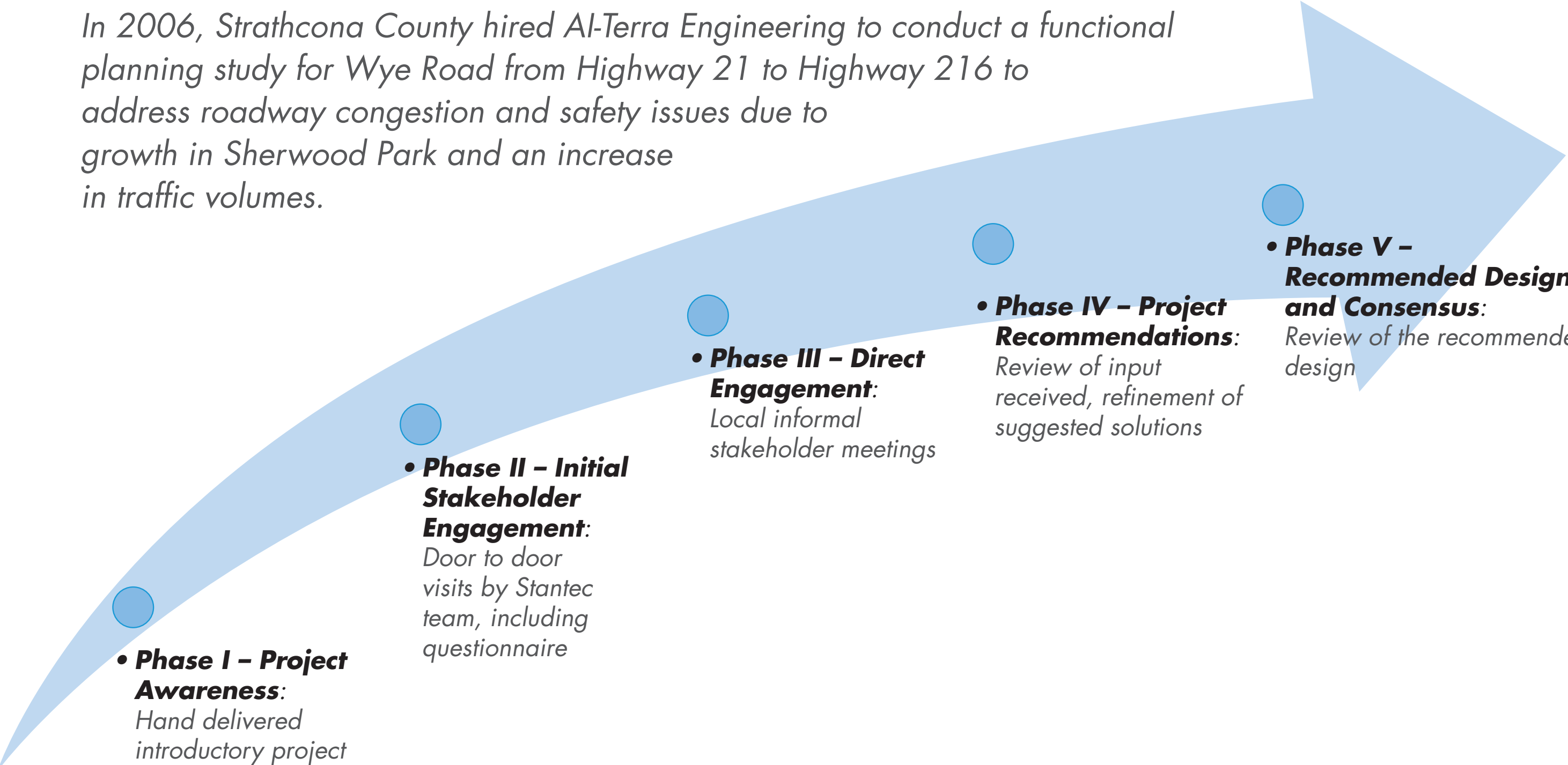
- Location: Sherwood Park & District Chamber of Commerce main boardroom (100 Ordze Avenue)
- Tuesday, March 12 - anytime between 3 PM – 7 PM
- Light refreshments will be provided

Thanks to the Sherwood Park & District Chamber of Commerce for providing us with their facility to host both meetings. We look forward to hearing from you!

(\* valued at \$525 +GST)

# Have your say about the next improvements on the Wye Road corridor!

In 2006, Strathcona County hired Al-Terra Engineering to conduct a functional planning study for Wye Road from Highway 21 to Highway 216 to address roadway congestion and safety issues due to growth in Sherwood Park and an increase in traffic volumes.



• **Phase I – Project Awareness:**  
Hand delivered introductory project

• **Phase II – Initial Stakeholder Engagement:**  
Door to door visits by Stantec team, including questionnaire

• **Phase III – Direct Engagement:**  
Local informal stakeholder meetings

• **Phase IV – Project Recommendations:**  
Review of input received, refinement of suggested solutions

• **Phase V – Recommended Design and Consensus:**  
Review of the recommended design

\*"Collaborate" is one of the four different degrees of public involvement outlined in the Strathcona County Public Engagement Policy. These levels, in order of increasing engagement, are Input, Listen & Learn, Collaborate, and Empower.



# Themes We Heard

## Access:

- Business access is crucial
- Accidents limit access
- Improve access – improve flows

## Safety

- Left turns are causing accidents
- “Crazy” intersection of Sherwood Drive
- People avoid Wye Road all together

## Landscaping:

- Enhance businesses
- Reduce speed
- Look appealing

## Bicycle and Pedestrian:

- Improve pedestrian access and mobility
- Mobility for older people
- Younger families moving in
- Area dynamic is changing

## Intersection Design:

- Signalization; i.e. timing, count down
- Pedestrian facilities

## Drainage:

- Consider utilities in future expansions

## Parking:

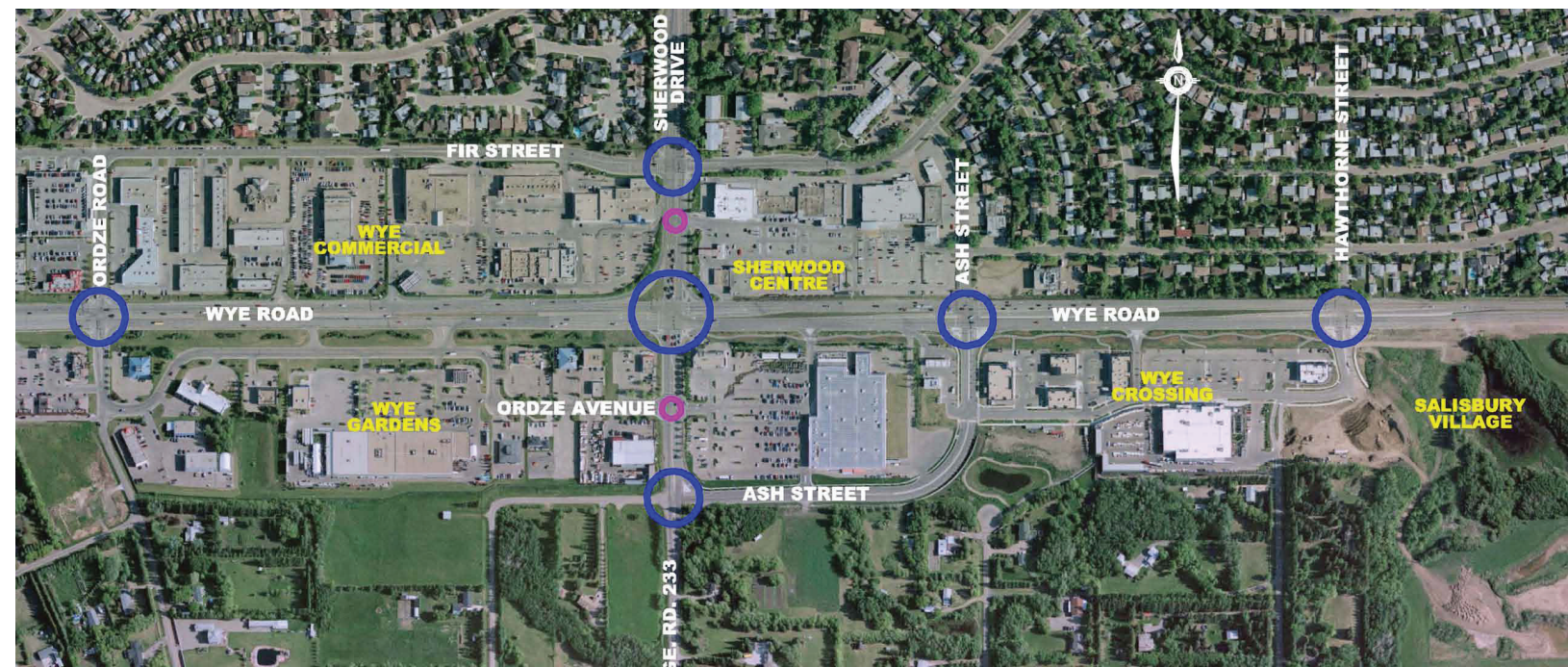
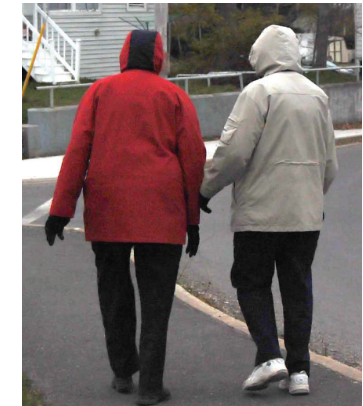
- Adequate parking for business patrons
- Transit Park and Ride is problematic

## General Comments:

- Not feasible to remove access to businesses
- Left turns cannot be removed
- In 20 years, density of site and adjacent land uses will increase

# Values We Heard

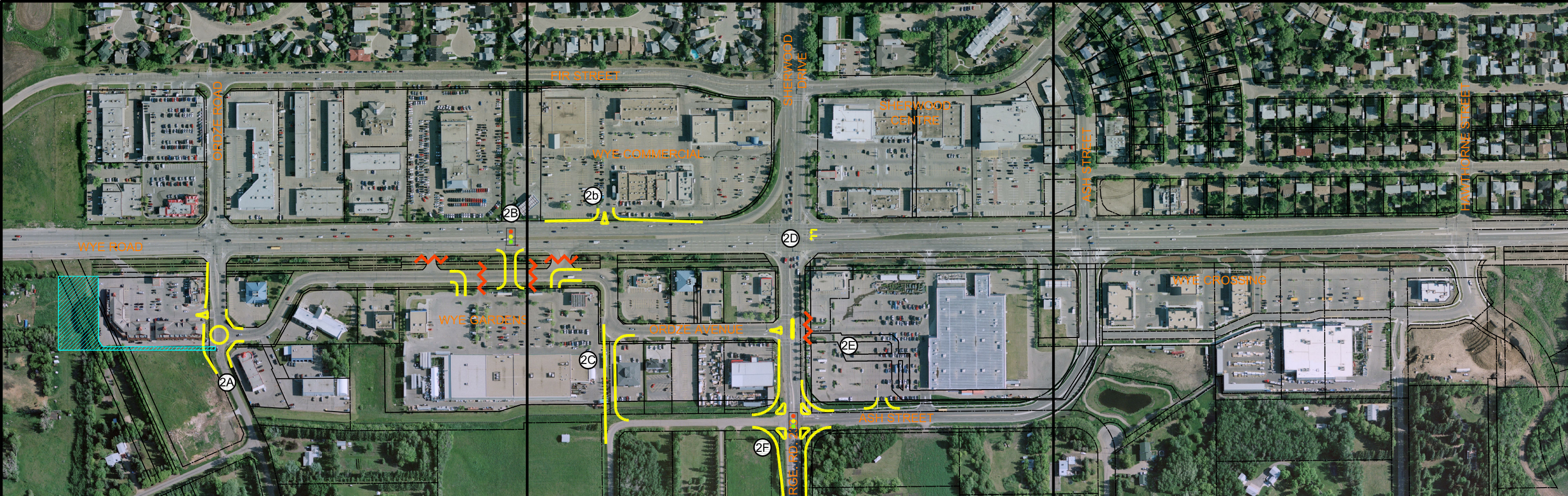
- Enhance business viability
- Overall safety
- Accommodate all users (pedestrian and bicycle)
- Accessibility



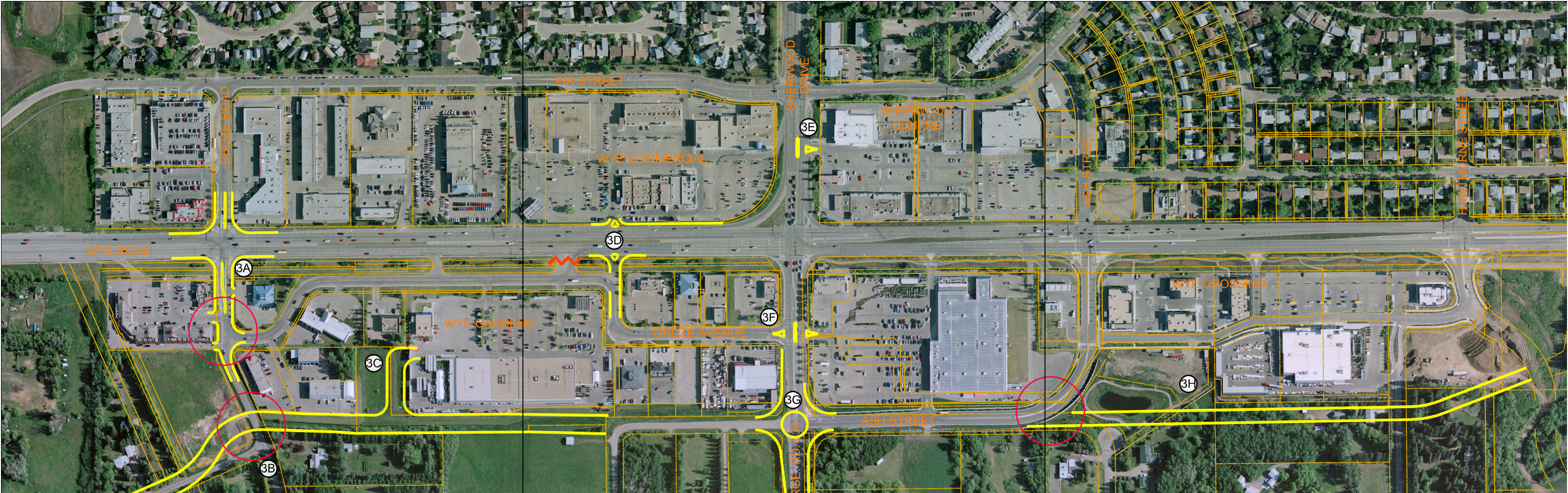






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OPTION			ADVANTAGES / BENEFITS	CHALLENGES	FULLY SUPPORT	SUPPORT WITH CONDITIONS	DO NOT SUPPORT	OPTION			ADVANTAGES / BENEFITS	CHALLENGES	FULLY SUPPORT	SUPPORT WITH CONDITIONS	DO NOT SUPPORT	OPTION			ADVANTAGES / BENEFITS	CHALLENGES	FULLY SUPPORT	SUPPORT WITH CONDITIONS	DO NOT SUPPORT
3A	<u>Widen Ordze Road</u>		- More capacity (two lanes) for north / south traffic through Ordze / Wye Intersection	- New allocation of road right-of-way from private land - Modified access to chamber of commerce - Challenging Intersection alignment with Ordze Avenue				3D	<u>New right-in / right-out access to Wye Road</u>		- Increase access	<u>North Side</u> - Grades may be challenging - Modifications to Wye commercial parking lot will be needed <u>South Side</u> - Weaving eastbound traffic across Wye Road to make left at Sherwood Drive combined with right turns at Sherwood Drive				3G	<u>Roundabout at Ash Street and Sherwood Drive</u>		- Improve intersection safety - Improve intersection flow - Improve lane alignment on Sherwood Drive	- New allocation of road right-of-way from private land			
3B	<u>Extend Ash Street west</u>		- Improve access and connectivity - Provide additional access to sites	- Challenging Intersection alignment with Ordze Road - New allocation of road right-of-way from private land - Connections along back of businesses - Residential to the south may be perceived to be adversely affected				3E	<u>Right-in / right-out to Sherwood Centre from Sherwood Drive</u>		- Reduces access to Sherwood Centre - All-directional access closed, right-in / right-out access opened - Improve safety - Reduce high risk turning movements - Reduce traffic queuing - Reduce traffic queuing	- Reduces access to Sherwood Centre - All-directional access closed, right-in / right-out access opened				3H	<u>Extend Ash Street east</u>		- Improve access and connectivity - Provide additional access to sites	- Challenging Intersection alignment with Ash Street (northbound) - New allocation of road right-of-way from private land - Water retention pond must be modified / relocated - Residential to the south may be perceived to be adversely affected			
3C	<u>Extend Ash Street north to Canadian Tire</u>		- Improve access and connectivity - Improve circulation	- Property impacts to Bank of Montreal and Canadian Tire - Parking lot modifications needed (as a minimum)				3F	<u>No left turns to Ordze Avenue and Walmart site</u>		- Improve safety - Reduce traffic queuing - Reduce traffic weaving - Increase traffic flow on Sherwood Drive and Wye Road	- Reduced access to Wye Gardens - Reduced access to Walmart, ATB and Payless Shoes											





**Stantec**

March 27, 2013

Dear Business and / or Property Owner,

**Reference: Wye Road Stakeholder Engagement**

As you may know, Stantec is working with managers, business owners, and property owners (stakeholders) to complete the last phase of the Wye Road Functional Study. Many of you have provided your insights and perspectives. We recently held meetings on March 7 and 12, to relay back what we have heard so far from the stakeholders and had 19 participants attend and share with us their perspectives and comments.

Attached are small versions of the three displays shown at the meetings. Please note these displays represent the comments we heard during our local business meetings from the stakeholders which took place in January. **These displays are not proposed plans or concepts.** The comments provided by the stakeholders in the meetings will help to refine the tools to be considered in the development of approaches for the long term planning for Wye Road.

All of the input we have received thus far from the stakeholders will be considered in the development of viable concepts for the long-term development of Wye Road. These concepts will be developed and then brought forward in open houses to be scheduled in mid to late April.

Thanks to the Sherwood Park & District Chamber of Commerce for providing us with their facility to host both meetings.

All the participants for both meetings were entered into a draw for a ¼ page advertisement in the Sherwood Park News (total value of \$525 + GST):

- As drawn by the Chamber of Commerce, **the lucky winner is:**

**Horse & Rider – Congratulations!!**

If you have any comments or questions, please contact [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com) (780-917-7321) or [Teneya.Gwin@Stantec.com](mailto:Teneya.Gwin@Stantec.com) (780-969-3227).

Kind regards,

**Shawn Bravender, B.Sc., RPP, MCIP, CET, IAP2-certified**

Associate, Urban Development - Planning

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10160 - 112 Street

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Ph: (780) 917-7321

[Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com)

RECIPE 1 <u>(not a proposed concept)</u>					
Option		Possible Advantages / Benefits	Possible Challenges	Support	Conditional Support Non-Support
	Signal modification	Signal modifications at all intersections can be reviewed.	May improve some movements at the expense of others, or at the expense of the general corridor flow.	86%	14%
1A	Extend Ash Street west	Improve access and connectivity Provide additional access to sites	New allocation of road right-of-way from private land Connecting along back of businesses Residential to the south may be perceived to be adversely affected	33%	66%
1B	New signalized intersection	Improve access and connectivity Reduce current access issues to businesses north of Wye Road Increase traffic through Wye Gardens Impact to Mazda dealership parking and access Right-in/right-out access closed, all-directional access opened	New allocation of road right-of-way from private land Potential relocation of Bank of Montreal (BMO) Increase traffic through Wye Gardens Impact to Mazda dealership parking and access Right-in/right-out access closed, all-directional access opened Ordze Avenue closed on either side of new all-directional access Review of intersection spacing to Ordze Road Left-in access to Safeway for east bound traffic modified to right-in/right-out access	66%	33%
1C	Wye Commercial southbound access closed	Improved safety for Sherwood Drive and Fir Street Improved traffic flow to onto Wye Road westbound	Reduced access to Wye Commercial	60%	40%
1D	Relocate westbound access to Sherwood Centre	Better access and exiting from site Improve safety Reduce dramatic crossings of Wye Road for left turns onto Sherwood Drive	Grades from Wye Road to commercial site may be challenging May be perceived to be not as direct to Petro Canada site	66%	33%
1E	No left turns to Ordze Avenue and Wal-Mart site	Reduce traffic queuing Reduce traffic weaving Increase traffic flow on Sherwood Drive and Wye Road Additional access to Wal-Mart, ATB, and Payless Shoes	Reduced access to Wye Gardens Modified access to Wal-Mart, ATB and Payless Shoes	20%	20% 60%

#### GENERAL COMMENTS PROVIDED

Relocate Lights to Sherwood Drive and Ash Street – Ash Street access is problematic.  
Commercial access has to be upgraded – safety first.  
Restricts access to my store.







RECIPE 2 (not a proposed concept)						
Option		Possible Advantages / Benefits	Possible Challenges	Support	Conditional Support	Non-Support
	Parking west of Cafferty's	Additional parking / access to business area	Pipeline company has control over approval	100%		
2A	Right-in/right-out	Improve intersection safety	Potential new allocation of road right-of-way from private land	33%	11%	56%
	Traffic Circle	Improve intersection flow	No access to business west of Ordze Road for northbound Ordze Road traffic			
2B	New signalized intersection on Wye Road	Improve direct access to Wye Commercial	North side - grades may be challenging North side - modifications to Wye Commercial parking lot will be needed	22%	33%	45%
2b	New right-in/right-out access to Wye Commercial	Right-in/right-out access closed, all-directional access opened	South side - modifications to Wye Gardens parking lot(s) will be needed Closure of Ordze Avenue, redirect traffic through Wye Gardens Review of intersection spacing to Ordze Road and Sherwood Drive	50%	50%	
2C	Extend Ash Street north to Ordze Avenue	Improve additional access	Significant property impacts to Expert Locksmith and Canadian Tire			
		Improve circulation	Potential relocation of Expert Locksmith	29%	14%	57%
			Parking lot modifications needed (as a minimum)			
2D	Westbound - left turns on Wye Road to Sherwood Drive	"May" improve westbound to southbound left turn at the expense of other movements or general corridor flow increase capacity for westbound to southbound traffic	"May" improve westbound to southbound left turn at the expense of other movements or general corridor flow Potential new allocation of road right-of-way from private land to accommodate additional southbound lanes	17%	50%	33%
2E	No left turns to Ordze Avenue and Wal-Mart site, provide additional access	Reduce traffic queuing	Reduced access to Wye Gardens			
		Reduce traffic weaving	Modified access to Wal-Mart, ATB and Payless Shoes	11%	45%	44%
		Increase traffic flow on Sherwood Drive and Wye Road				
		Additional access to Wal-Mart, ATB, and Payless Shoes				
2F	Signalized intersection of Sherwood Drive and Ash Street	Improved intersection function	Potential new allocation of road right-of-way from private land	45%	44%	11%
		Increased number of lanes on Sherwood Drive/Range Road 233				

#### GENERAL COMMENTS PROVIDED

- 2A. Widen Ordze Road (south of Wye Road). – 2X
- 2B. Wye Garden's west – need another outlet.
- 2D. Conditional on widening Sherwood Drive (3 lanes each direction). Not needed due to Ash intersection.
- 2E. Lots of customer flow from Wal-Mart to Ordze Avenue. If access is closed (Wal-Mart), provide left hand turn to Ordze Avenue. – 2X
- 2F. Do not like pedestrian islands. So long as volumes and support...





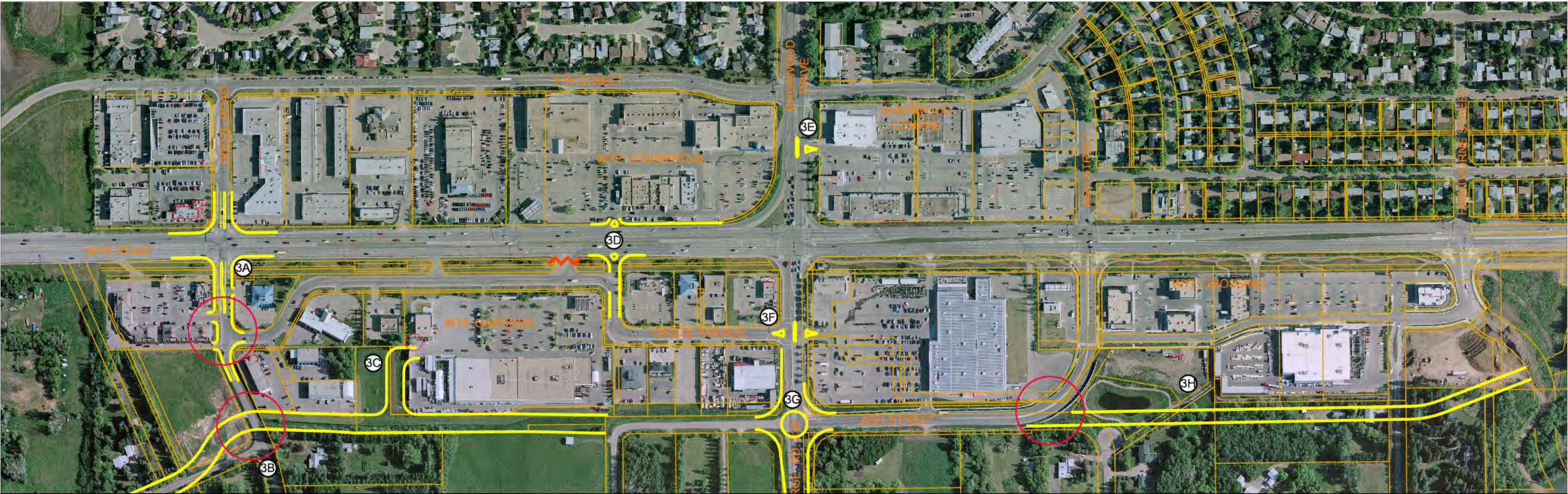


RECIPE 3 <u>(not a proposed concept)</u>						
Option		Possible Advantages / Benefits	Possible Challenges	Support	Conditional Support	Non-Support
3A	Widen Ordze Road	More capacity (two lanes) for north / south Traffic through Ordze / Wye intersection	Potential new allocation of road right-of-way from private land Modified access to the Chamber of Commerce, perhaps additional access Challenging intersection alignment with Ordze Avenue	100%		
3B	Extend Ash Street west	Improve access and connectivity Provide additional access to sites	Challenging intersection alignment with Ordze Avenue New allocation of road right-of-way from private land Connecting along back of businesses Residential to the south may be perceived to be adversely affected	29%	29%	42%
3C	Extend Ash Street north to Canadian Tire	Improve additional access / connectivity Improve circulation	Potential property impacts to Bank of Montreal and Canadian Tire Parking lot modifications needed (as a minimum)	20%		80%
3D	New right-in/right-out access to Wye Road	Increase access	North side - grades may be challenging North side - modifications to Wye Commercial parking lot will be needed South side - Weaving eastbound traffic across Wye Road to make left at Sherwood Drive combined with right turns at	33%	66%	
3E	Right-in/right-out to Sherwood Centre from Sherwood Drive	Improve safety Reduce high risk turning movements Reduce traffic queuing Reduce traffic weaving	Reduces access to Sherwood Centre All-directional access closed, right-in/right-out access opened	50%	50%	
3F	No left turns to Ordze Avenue and Wal-Mart site	Improve safety Reduce traffic queuing Reduce traffic weaving Increase traffic flow on Sherwood Drive and Wye Road	Reduced access to Wye Gardens Modified access to Wal-Mart, ATB and Payless Shoes	11%		89%
3G	Traffic circle at Ash Street and Sherwood Drive	Improve intersection safety Improve intersection flow Improve lane alignment on Sherwood Drive	Potential new allocation of road right-of-way from private land	25%	25%	50%
3H	Extend Ash Street east	Improve access and connectivity Provide additional access to sites	Challenging intersection alignment with Ash Street (northbound) New allocation of road right-of-way from private land Water retention pond must be modified/relocated Residential to the south may be perceived to be adversely affected		17%	83%

#### GENERAL COMMENTS PROVIDED

3D. Another signalized intersection is needed.  
 3E. Need to provide another entrance (Sherwood Drive south) into Sherwood Center. Agree – dangerous turn needs to be removed, need a better access point.  
 3F. It should be closed off – see 2E.  
 3H. There should be long range consideration.







Appendix V

# Design Concepts

May 2, 2013

Dear Business and/or Property Owners

**Reference:** Wye Road Stakeholder Engagement – Stakeholder meetings

As you may know, Stantec is working with managers, business owners, and property owners to complete the last phase of the Wye Road Functional Study. Many of you have provided your insights and perspectives. We have scheduled two meetings to relay what we have heard so far in the form of three (3) potential design concepts (see attached).

The meetings are a drop-in format so feel free to come for as long as you would like.

Here are the details:

- Location: Sherwood Park United Church (20 Fir Street)
- Thursday, May 9 - drop in anytime between 3 PM – 7 PM, and
- Tuesday, May 14 - drop in anytime between 3 PM – 7 PM
- Light refreshments will be provided

Thanks to the Sherwood Park United Church for providing us with their facility to host both meetings. We look forward to hearing from you!

If you have any comments or questions, please contact [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com) (780-917-7321) or [Teneya.Gwin@Stantec.com](mailto:Teneya.Gwin@Stantec.com) (780-969-3227).

*\*Your email address will ONLY be used to transfer project information and updates as well as coordinate local meeting times.*

If you would like to be removed from this email list, please let us know at [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com)

Kind regards,



**Shawn Bravender, B.Sc., RPP, MCIP, CET, IAP2-certified**

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## **CONCEPT 1**

### **First Choice – 0**

- No comments

### **Second Choice - 2**

- Wendy's needs all directional access off Sherwood Drive. Close down Wal-Mart access of Sherwood Drive to fix traffic issues. All 3 concepts do not work for Wendy's. Wendy's business will be ruined!
- I don't like that this no longer allows a left turn from Ordze onto Sherwood Drive.
- Closing 233 to Ordze. Closing Ordze to Wal-Mart NOT acceptable.
- Concept 1 will hurt a lot of businesses on Ordze Avenue. None of the concepts work, smaller businesses are being hurt due to traffic issues created by Wal-Mart. Just provide Wal-Mart access from Ash Street. Close it's access off Sherwood Drive.

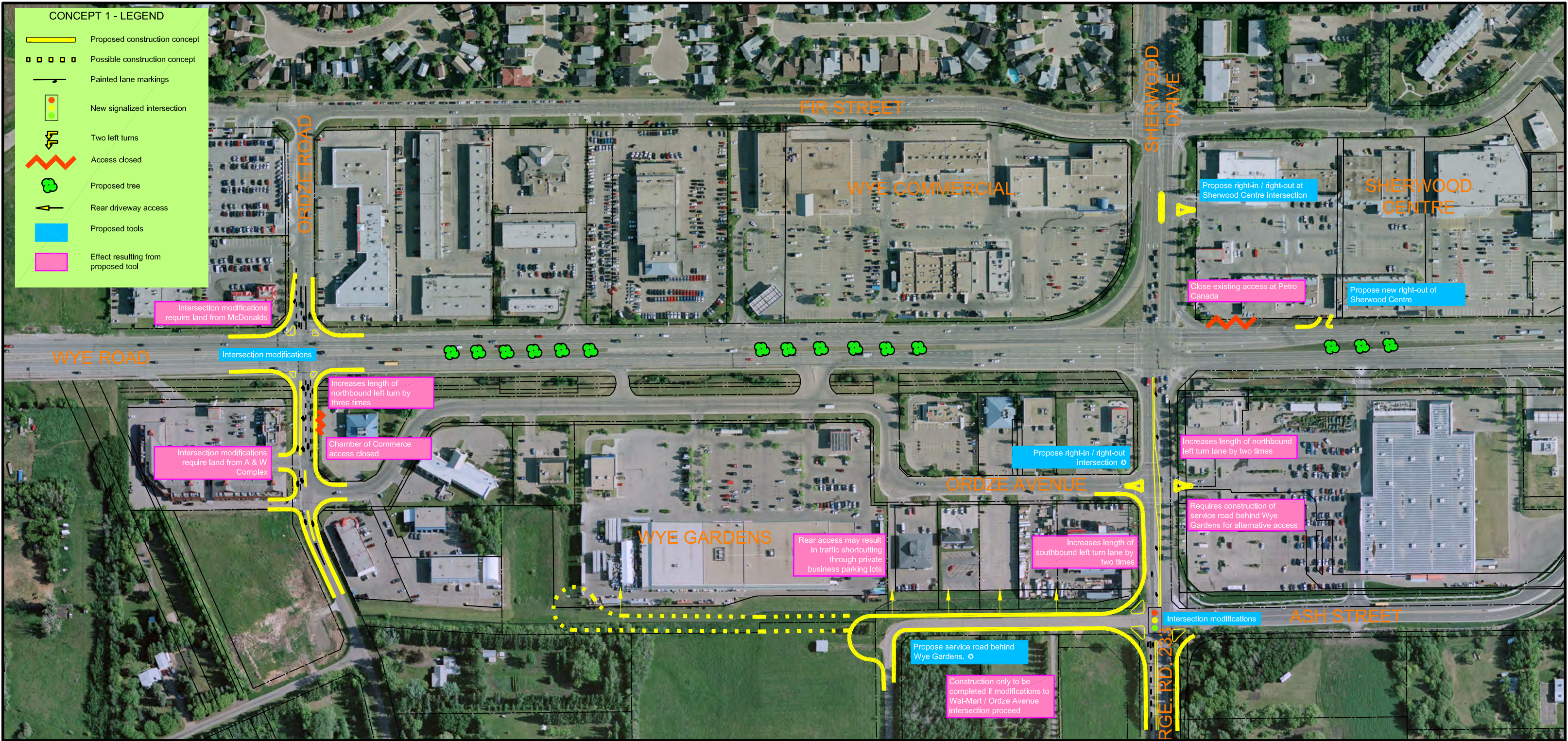
### **Concerns:**

- Left turn from RGE RD 233 to Ordze Avenue.
- All concepts any interference to traffic flow at 233 and Ordze is not acceptable.

### **Additional Comments:**

- At Wye/Ordze need free flow merge lane for (don't have to pull into straight thru lane) for SB to WB traffic.
- Improve visibility of Rin/Rout access to Wal-Mart off Wye.
- Not enough proposed. Like Concept 2 and 3 combination better.
- Extend Ash Street east.







## CONCEPT 2

### First Choice – 15

- Proposed Ordze Road and Ash Street intersection requires some type of signage or warning lights (i.e.: SLOW, or SPEED LIMIT 30, or STOP/YIELD, flashing warnings). Same applies to A&W and Chamber of Commerce on Ordze Avenue.
- Improvements at Wye Road and Ordze (A&W) are helpful.
- Ok to close Chamber entrance/exit. Gives us more Parking.
- Respectively keep the "separation" between the country residential and the commercial/businesses via bound barrier fencing and/or berm.
- Add road on north of Wye between Mazda and 2nd Cup same as Concept 3.
- On Concept 2 Mazda roadway should be added.
- Appreciate the meridians extended on Ordze Road and anything to slow down traffic for safety reasons.
- 1st choice with exception of widening intersection on BP's side. Do not want to lose as much Blvd. space.
- All 3 concepts: Sherwood Center Dental Clinic is very concerned about the traffic taking a short cut thru our parking lot which is on Fir Street and driving towards Shoppers and Petro Can. The "back way" as some do already!! Safety! (we have small kids coming to office)

### Second Choice - 0

- Concept 2 will damage Wendy's business. Most of the traffic is caused by Wal-Mart and it is not fair to penalize other businesses. Close Wal-Mart's access on Sherwood Drive and create an access for them on Ash Street - problem solved!
- Closed off London Drugs access, that's where accidents are caused.
- Close down access to Wal-Mart off Sherwood Drive as they currently have 3 accesses. This will alleviate traffic concerns on Sherwood Drive. In all 3 Concepts Wendy's is affected severely!
- Keep access to London Drugs!

### Concerns:

- Ash Street extension is a must.
- Left turn from RGE RD 233 to Ordze Ave.
- Signalized Canadian Tire access from #3 added to # 2.
- Access from Ash Street to Earls.

### Additional Comments:

- Dedicated right turn for southbound to westbound at Ordze Road to allow buses to turn without being blocked by through traffic (all concepts).
- Extend Ash Street east behind Rona.



CONCEPT 2 - LEGEND

- Proposed construction concept
- Possible construction concept
- Painted lane markings
- New signalized intersection
- Two left turns
- Access closed
- Proposed tree
- Rear driveway access
- Proposed tools
- Effect resulting from proposed tool

WYE ROAD

ORDZE ROAD

FIR STREET

WYE COMMERCIAL

SHERWOOD DRIVE

SHERWOOD CENTRE

WYE GARDENS

ORDZE AVENUE

ASH STREET

RD 23

Intersection modifications require land from McDonalds

Intersection modifications

Intersection modifications require land from Boston Pizza Complex

Propose two lanes for both northbound and southbound

Intersection modifications require land from A & W Complex

Increases length of northbound left turn by three times

Chamber of Commerce access closed

Possible area for additional parking. Subject to relocation of storm pond

Possible new storm pond

Close or relocate access would be desirable due to intersection changes at Sherwood / Wye

Propose right-in / right-out at Sherwood Centre intersection

Close existing access at Petro Canada

Concern exiting vehicles will cut across Wye to turn left at Sherwood Drive

Propose new right-out of Sherwood Centre

Propose right turn lane

Additional land required from Wendy's

Propose right-in / right-out intersection

Propose two left turns at Wye/Sherwood intersection for southbound and westbound

Increases length of northbound left turn lane by two times

Requires construction of service road behind Wye Gardens for alternative access

Connection road to Ordze Avenue.

Requires land from Canadian Tire

Increases length of southbound left turn lane by two times

Propose service road behind Wye Gardens.

Construction only to be completed if modifications to Wal-Mart / Ordze Avenue intersection proceed

Intersection modifications



## CONCEPT 3

### First Choice – 6

- No comments

### Second Choice - 9

- Don't want a road between Mazda and Smilie's (*commenter has since changed position*)
- Suggest Ash Street expansion as in Concept 1
- Proper access by Mazda good idea.
- Since traffic on Wye Road is an issue, more lights are unlikely to be helpful.
- Closing Ordze Ave to northbound and Wal-Mart traffic is UNACCEPTABLE.
- Better signal for Sherwood Drive Fir Street corner - look at blocking off the London Drugs turn off.
- Concept 3 will not work for Wendy's Restaurant. Customers south on Range Road 233 have to go through a longer route to get into Wendy's. Restriction on flow of traffic will destroy Wendy's business. The easiest and cost effective solution is to close access to Wal-Mart on Range Road 233 (Sherwood Drive) and create new access on Ash Street. Wendy's will absolutely NOT agree to appropriation of land for widening lanes.
- All directional access is required for Wendy's. The current proposals will hurt the restaurant. Wal-Mart is main contributor of traffic and therefore access to Wal-Mart off Sherwood Drive should be closed. The current proposals will destroy Wendy's business!

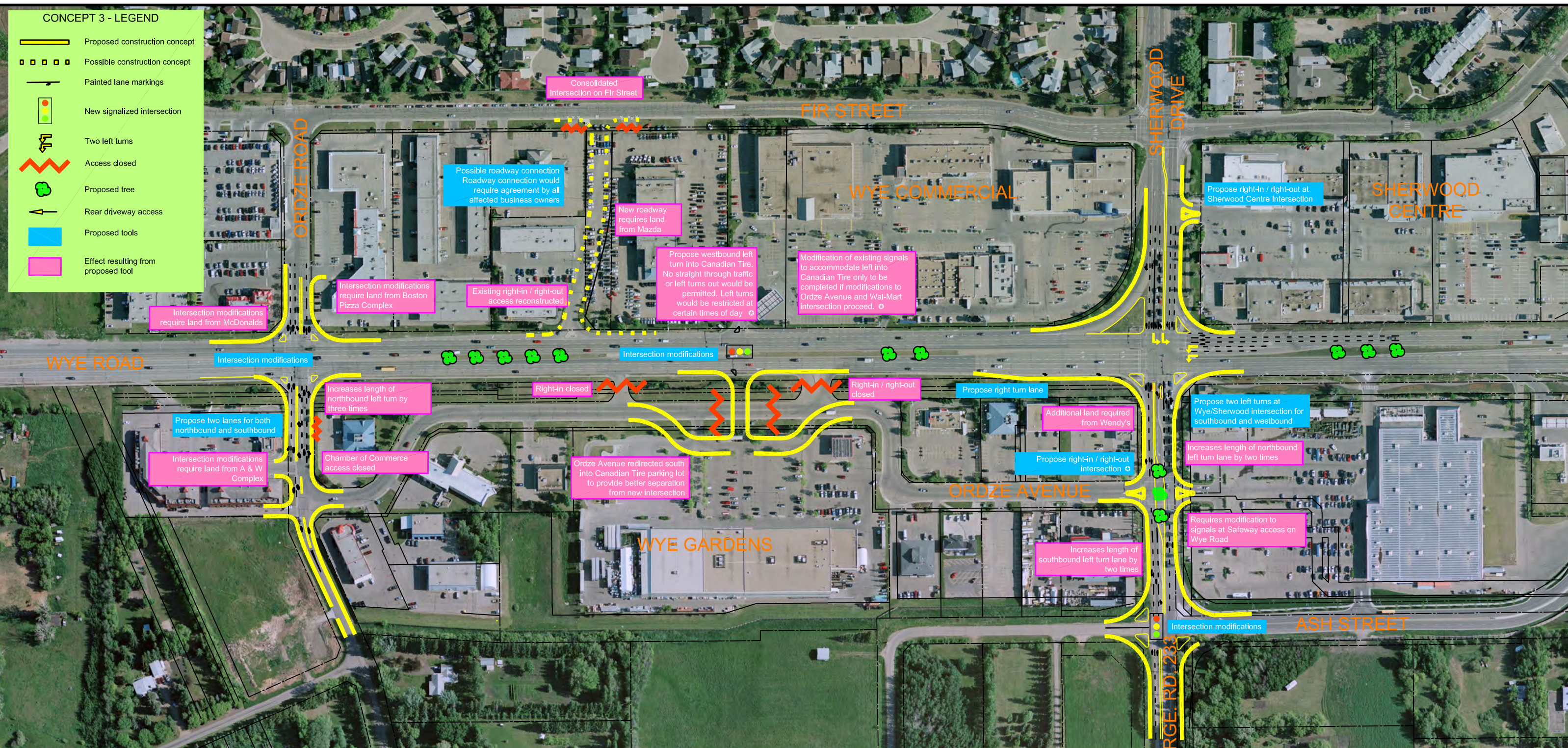
### Concerns:

- Add to Ash Street connection from Ordze to Range Road 232.
- Left turn from Range Road 233 to Ordze Ave.

### Additional Comments:

- I like turning Mazda parking lot/2nd Cup into roadway.
- Right turn lane from Wye to NB Ordze.
- Add another traffic light on Wye Road cause more tie-ups in traffic flow.







Appendix VI

# Final Recommendation



**Stantec**

June 4, 2013

Dear Business and / or Property Owner,

**Reference: Wye Road Stakeholder Engagement**

As you may know, Stantec is working with managers, business owners, and property owners to complete the last phase of the Wye Road Functional Study of which many of you have provided your insights and perspectives. Directly resulting from the input received throughout this project from the stakeholders, a proposed design concept has taken form (see attached). We have scheduled two meetings to showcase the potential design concept. After considering comments received during the two upcoming meetings, and subsequent concept revisions, we will be submitting the final design to the County and recommending it for approval.

The meetings are a drop-in format so feel free to come for as long as you would like.

Here are the details:

- Location: Sherwood Park United Church (20 Fir Street)
- Wednesday, June 12 - drop in anytime between 3 PM – 7 PM, and
- Tuesday, June 25 - drop in anytime between 3 PM – 7 PM
- Light refreshments will be provided

Thanks to the Sherwood Park United Church for providing us with their facility to host both meetings. We look forward to hearing from you!

If you have any comments or questions, please contact [Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com) (780-917-7321) or [Teneya.Gwin@Stantec.com](mailto:Teneya.Gwin@Stantec.com) (780-969-3227).

Kind regards,

**Shawn Bravender, B.Sc., RPP, MCIP, CET, IAP2-certified**

Associate, Urban Development - Planning

Stantec

10160 - 112 Street

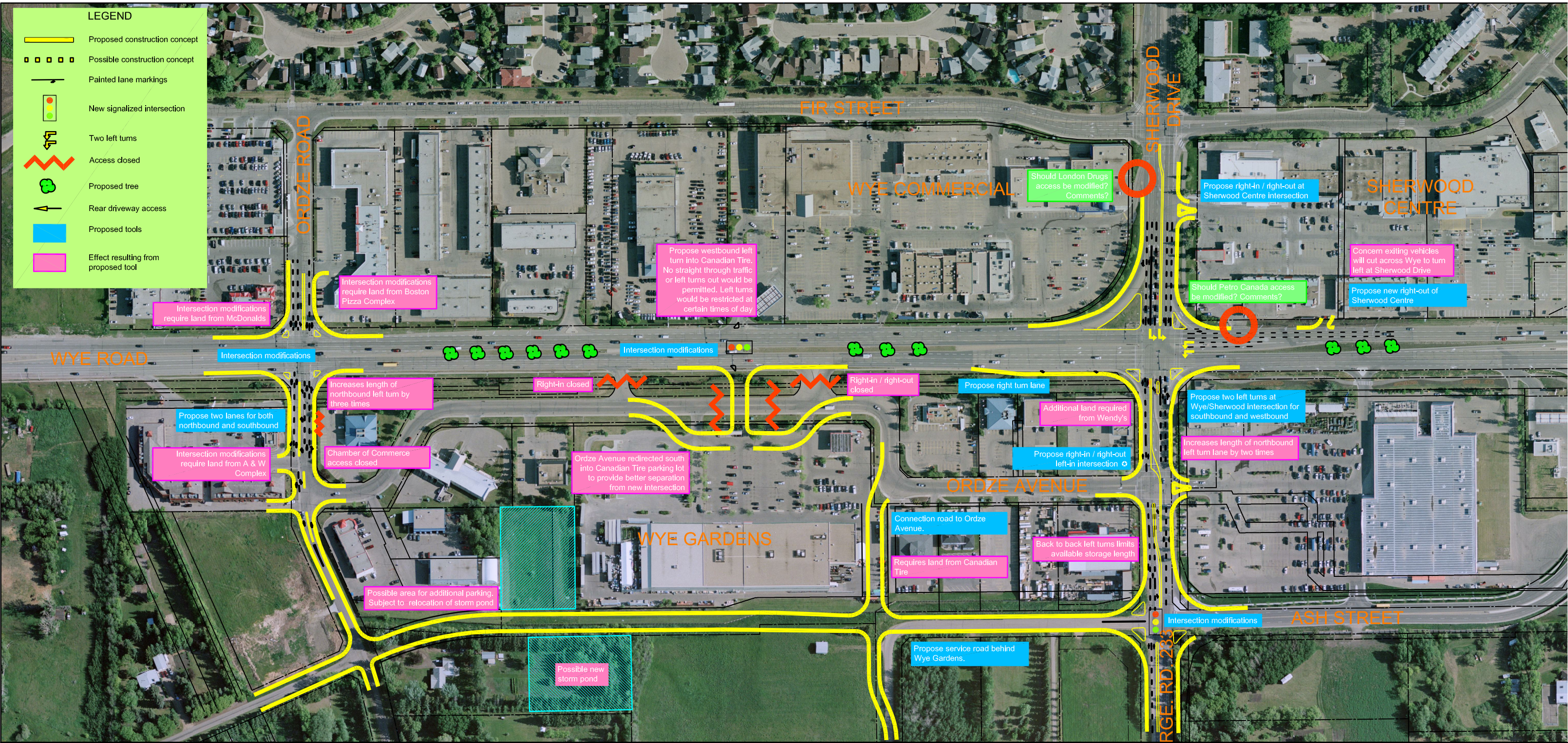
Edmonton AB T5K 2L6

Ph: (780) 917-7321

[Shawn.Bravender@Stantec.com](mailto:Shawn.Bravender@Stantec.com)

- Canadian Tire Access – Landscaping can't block view of businesses
- Intersection of Sherwood Drive and Ash Street is long overdue. Looks good!
- Concern corner at Canadian Tire, Expert Lock (entrance to Expert Parking)
- Extension of Ash Street West to Ordze Road is a good idea.
- Closure of Chamber Driveway is good. Provides more space for parking.
- Remove Right in/Right out at Wal-Mart
- Remove Left turn at Ordze Avenue
- Bypass Road is a simple solution
- Shift Canadian Tire access east to Dairy Queen. No Left in.
- Drop/setback nose or and median to allow safer left turns and not force a sharp radius connection at end of turn. (Brian K)
- Canadian Tire, Safeway Intersection - Good idea
- Looks much better. Thank you for considering our feedback. Johnny
- London Drugs access stays open!
- Much better than last concept. Still believe Wal-Mart does not need access off RR233. Wendy's would benefit from all directional access off RR 233/Sherwood Drive
- Right in/Right out on Sherwood Drive to Sherwood Centre - Good idea!
- Optional access from Ash Street to Earl's parking lot?
- Left hand turn onto Ordze Avenue is a good idea.
- Some improvement however it is NOT perfect. Wal-Mart should get access of Ash Street only-not off Sherwood Drive (Range Road 233). Also unclear how additional land will be acquired from businesses - There is not much available.
- 😊
- Looks good guys! (Barry)
- Queuing space is increased for left turns onto Fir Street from North Bound on Sherwood Drive. Looks good.
- Much better access to Ordze Avenue than other proposals
- Any closing of Ordze Avenue NOT acceptable
- Ash and Sherwood Intersection is good
- Looks good to me!
- Looks good for traffic flows







Appendix VII

# Master Stakeholder Engagement Database

WYE Road Master Stakeholder Engagement Database				Phase I	Phase II				Phase III	Phase IV	Phase V	Phase VI
NAME	BUSINESS ADDRESS			Project Kick Off	Questionnaire Distribution	Questionnaire Return/ Participate	Comments		Face-to-Face Meetings	Stakeholder Feedback Sessions	Design Concepts	Final Recommendation
1053163 Alberta Limited		983	Fir St.		Oct. 24, 2012							
1350025 Alberta Ltd.	109	977	Fir St.		Oct. 24, 2012							
1549933 Alberta Ltd.		279	Wye Rd.									
1611474 Alberta Ltd.	201	979	Fir St.		Oct. 24, 2012							
1611474 Albverta Ltd.	210	979	Fir St.		Oct. 24, 2012							
5 Guys Burgers and Fries	60	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012							
654210 Alberta Ltd.		193	Ordze Ave.		Oct. 24, 2012							
698198 Alberta Ltd.	205	979	Fir St.		Oct. 24, 2012							
7838760 Canada Inc.		1000	Alder Ave.		Oct. 25, 2012							
7838760 Canada Inc.	985	999	Fir St.		Oct. 24, 2012							
799766 Alberta Ltd.		916B	Ash St.		Oct. 25, 2012							
899992 Alberta Ltd.		910	Ash St.		Oct. 25, 2012							
A & D Child Care Centre & Out of School Care	101	957	Fir St.	Sept. 20, 2012	Oct. 25, 2012							
A & W Restaurant (Wye Road)	43	99	Wye Rd.	Sept. 19, 2011	Oct.24, 2012		Y	January 30, 6:00 pm at Advico.	Mtg #4 - Jan 31			
Aerotek Computers		918	Ash St.		Oct. 25, 2012	Y	Y	Left message				25-Jun
Alltax & Accounting Inc.	200	937	Fir St.	Sept. 19, 2011	Oct. 25, 2012							
Angus H. Thompson Executive for Mary Thompson												
Anthony Marie Gifts		916	Ash St.	---	Oct. 25, 2012			Call back - too busy				
ATB Financial --Wye Road		201	Wye Rd.	Sept. 17, 2012	Oct. 24, 2012			January 30, 1:00 pm Rona Meeting Room 301 Wye Road	Mtg #3 - Jan 30			
ATCO Gas and Pipelines Ltd. (Gas)												
B Royale Limousine Service	109	957	Fir St.	Sept. 20, 2012	Oct. 24, 2012							
Bernie's Cellular		971A	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012							
BMO Bank of Montreal		145	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	NO					
Booster Juice	18	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012							
BOSS Liquor		916A	Ash St.	---	Oct. 25,2012			Left message on both #'s				
Boston Pizza		967	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012							
Boyd's Seafood	103	979	Fir St.	Sept. 20, 2012	Oct. 24, 2012							
Brentwood Animal Hospital		185	Ordze Ave.	Sept. 20, 2012	Oct. 24, 2012	Y	Y	Jan 28 with Expert Lock	Mtg #1 - Jan 28	Mar 7 & Mar 12	14-May	12-Jun
Brentwood Chiropractic Clinic	108	937	Fir St.	Sept. 19, 2011	Oct. 25, 2012	Y	NO					
Buffet Royale Carvery	109	957	Fir St.	Sept. 20, 2012	Oct. 24, 2012							
Buster's Pizza Donair & Pasta	127	120	Wye Rd.	Sept. 19, 2011	Oct.24, 2012							
Caffrey's in the Park		99	Wye Rd.	Sept. 17, 2012	Oct.24, 2012							
Cali Nails	84	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012	Y	?	left #. Owner will CB				
Canadian Driveline Products Inc. (AKA HART Transmission)		110	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	NO					
Canadian Imperial Bank of Commerce CIBC	250	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012	Y	Y	Left message on January 31. Interested in attending Thursday January 31 meeting	Mtg #5 - Jan 31			
Canadian Tire Associate Store # 428		169	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012			Jan 28 with Expert Lock	Mtg #1 - Jan 28	Apr 9 - meeting		
Canadian Tire Gas Bar		169	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012			catch in the AM				
Car Finesse	107	977	Fir St.	Sept. 20, 2012	Oct. 24, 2012							
Cathy Olesen, MLA-Sherwood Park	116B	937	Fir St.	Sept. 19, 2012	Oct. 25, 2012							
Chocolate Blue Hair Company	17	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012	Y	NO					
Cobs	160	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012							
Color Me Mine	128	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012							
Crimpz Beads	21	99	Wye Rd.		Oct. 24, 2012							
Dairy Queen		192	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	?	Jan 28 with Expert Lock	Mtg #1 - Jan 28			

WYE Road Master Stakeholder Engagement Database				Phase I	Phase II				Phase III	Phase IV	Phase V	Phase VI
NAME	BUSINESS ADDRESS			Project Kick Off	Questionnaire Distribution	Questionnaire Return/ Participate		Comments	Face-to-Face Meetings	Stakeholder Feedback Sessions	Design Concepts	Final Recommendation
Dan's Automotive	109	977	Fir St.	Sept. 20, 2012	Oct. 24, 2012							
Dave Quest MLA	19	99	Wye Rd.	Sept. 17, 2012	Oct.24, 2012					12-Mar		
Definitive Tech Service	33	99	Wye Rd.	Sept. 17, 2012	Oct.24, 2012							
Dispensaries (1991) Limited		937	Fir St.		Oct. 25, 2012			not interested				
Dollarama	132	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012			Owner to CB				
Domo Gas	10605		106 Ave.		Oct. 25, 2012							
Don McKay Realty		955	Ordze Rd.		Oct. 25, 2012							
Dr. Bernstein Diet & Health Clinics		1020	Wye Rd.	Sept. 18, 2012	Oct. 24, 2012			Left message				
Dulux Paints		196	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012			Jan 28 with Expert Lock				25-Jun
Earl's Restaurants Sherwood Park		194	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	Y	Jan 28 with Expert Lock	Mtg #1 - Jan 28		9-May x3	June 12 x 3
Eastgate Physical Therapy	100	937	Fir St.	Sept. 19, 2012	Oct. 25, 2012							
Edo Japan (www.edojapan.com)	53	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Edward-Jones- Sean Mullane, CFP	290	664	Wye Rd.		Oct. 25, 2012							
Elk Island Public Schools		683	Wye Rd.		Oct. 25, 2012							
Energy Works/Alberta Reiki Centre	2 floor	912	Ash St.		Oct. 25, 2012							
Expedia CruiseShipCenters		967A	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012	Y	NO					
Expert Lock Services Ltd.		183	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	Y	Jan 28 with Expert Lock	Mtg #1 - Jan 28	Mar 7 & Mar 12; Apr 9 meeting	9-May & 14-May	12-Jun
Extreme Pita	24	993	Fir St.	Sept. 18, 2012	Oct. 24, 2012							
Eye-Bar Opticians and Sunglass Specialists	25	993	Fir St.	Sept. 18, 2012	Oct. 24, 2012							
Fabutan Suntan Studios	89	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012							
Faithful Jeweller		969A	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012							
Fir Street Bottle Depot	201	979	Fir St.	Sept. 20, 2012	Oct. 24, 2012			Call back Monday				
First Capital Holdings (ALB) Corp.		1000	Alder Ave.		Oct. 25, 2012				Mtg #6 - Feb 5 x2	email Apr 19	14-May	email June 11
First Capital Holdings (ALB) Corp.	985	999	Fir St.		Oct. 24, 2012			special session - Feb 5 requested				
First Capital Realty Inc.												
Foothills Equipment Liquidation Co. Ltd.		22355C	Wye Rd.		Oct. 25, 2012							
For Your Eyes Only Opticals Ltd.	110	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012			January 30, 1:00 pm Rona Meeting Room 301 Wye Road		Mar 7 - 2 came		
Fortier Creative Designs Inc.		930	Ash St.		Oct. 25, 2012			no #				
Frederick & Company LLP	206	937	Fir St.	Sept. 19, 2012	Oct. 25, 2012							
Fusion Music Inc.	139	120	Wye Rd.	Sept. 19, 2012	Oct.24, 2012		Y	January 29, 1:00 PM Training Room Booked Sparks Heating and Air Conditioning 115 Ordze Ave	Mtg #2 - Jan 29			email June 5
Get Real Natural Health- Beauty Within		1020	Sherwood Dr.		Oct. 25, 2012							
H & R Block	60	1020	Sherwood Dr.	CLOSED	Oct. 25, 2012							
Hape International Inc.	2nd floor	912	Ash St.		Oct. 25, 2012			does not affect business				
HART Transmission (AKA Canadian Driveline Products Inc.)		110	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	NO					
Herbal Magic	11	99	Wye Rd.	Sept. 17, 2012	Oct.24, 2012							
Home Hardware Building Centre		197	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012			Jan 28 with Expert Lock				12-Jun
Hoppe Holdings		99	Wye Rd.	---	Oct. 24, 2012					7-Mar	14-May	
Horse & Rider (formerly Equestrian Supplies)		196A	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	Y	Sent new owner a message		12-Mar		
Huku	180	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012			no-mtg location?				
Humford Management		945	Ordze Rd.	Sept. 20, 2012				January 29, 1:00 PM Training Room Booked Sparks Heating and Air Conditioning 115 Ordze Ave	Mtg #2 - Jan 29	12-Mar	14-May	
Information and Volunteer Center		100	Ordze Ave.	Sept. 17, 2012	Oct. 25, 2012							





WYE Road Master Stakeholder Engagement Database				Phase I	Phase II				Phase III	Phase IV	Phase V	Phase VI
NAME	BUSINESS ADDRESS			Project Kick Off	Questionnaire Distribution	Questionnaire Return/ Participate	Comments		Face-to-Face Meetings	Stakeholder Feedback Sessions	Design Concepts	Final Recommendation
Park Mazda		983	Fir St.	Sept. 19, 2012	Oct. 24, 2012	Y	Y	Cancelled January 30. Need to follow-up		Apr 17 meeting		
Payless Shoes		203	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012			Left message w/ staff @ ND - Jan 11 2013				
Pennington's	65	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012			January 30, 1:00 PM 301 Wye Road	Mtg #3 - Jan 30			
Petro Canada	10	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012			Left message				SUNCOR Letter / Email June 11 Petro-Can phone call approx. June 6
Picture This Framing & Gallery		959	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012					25-Jun		25-Jun
Planet Beach	108	957	Fir St.	Sept. 20, 2012	Oct. 25, 2012	Y	NO					
Presto Music		922	Ash St.		Oct. 25, 2012							
Pure Orthodontics		1048	91 St.		Oct. 25, 2012							
Quiznos	100	1000	Alder Ave.	Sept. 17, 2012	Oct. 25, 2012							
R.E Profession Corp.				Sept. 19, 2012								
RBC Royal Bank (Wye Road Crossing)	31	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Registry on Wye Ltd.	25	99	Wye Rd.	Sept. 17, 2012	Oct.24, 2012							
Reitman's	80	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012			No longer available				
Rexall Drug Store	11	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Richard A. McBain Executor for Marian E. McBain												
Richards Donair and Subs	130	99	Wye Rd.	Sept. 17, 2012	Oct.24, 2012	Y	?					
Ricky's Grill	170	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012							
Rona Home Centre #239	81	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012			HOST January 30, 1:00 PM Rona Meeting Room 301 Wye Road	Mtg #3 - Jan 30			
Royal Lepage Noralta Real Estate		955	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012						9-May	
Royal West Property Corp.	8	23333	Wye Rd.		Oct. 25, 2012							
Royop Development Corp	65	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Rudolf Glanz	13	23333	Wye Rd.		Oct. 25, 2012							
Safeway		985	Fir St.		Oct. 24, 2012			new store mgr - Feb			9-May x 2: email May 31	
Sanderss, Jossea & Coddie	98	23333	Wye Rd.		Oct. 25, 2012			January 30, 1:00 pm Rona 301 Wye Road	Mtg #3 - Jan 30	7-Mar	14-May	
Save On Foods		81	Fir St.	Sept. 18, 2012	Oct. 25, 2012			January 30, 1:00 pm Rona Meeting Room 301 Wye Road	Mtg #3 - Jan 30			
Scotiabank	15	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012	Y	NO					
Second Cup Wye Road	113	120	Wye Rd.	Sept. 19, 2012	Oct.24, 2012	Y	Y	January 30, 6:00 pm at Advico. Confirmed.	Mtg #4 - Jan 31			
Services for Children	203	937	Fir St.	Sept. 19, 2012	Oct. 25, 2012							
Servus Credit Union	101	301	Wye Rd.	Sept. 17, 2012	Oct. 24, 2012							
Shell		23263	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Sherwin Williams	21	993	Fir St.	Sept. 18, 2012	Oct. 24, 2012							
Sherwood Bowl Ltd.	301	975	Fir St.	Sept. 20, 2012	Oct. 24, 2012	Y	?					
Sherwood Centre Dental Clinic	120	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012						14-May	
Sherwood Exhaust Ltd.	204 206	979	Fir St.		Oct. 24, 2012			Might be able to make an evening meeting. Need to phone back with a specific date and time.				
Sherwood Grill	86	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012							
Sherwood Motorcycle and Power Products Ltd.		930	Ash St.		Oct. 25, 2012							
Sherwood Park & District Chamber of Commerce		100	Ordze Ave.	Sept. 17, 2012	Oct. 25, 2012			Jan 28 with Expert Lock x2	Mtg #1 - Jan 28 x2	Mar 12 - 5 came	14-May x 2	Jun 12 x 2
Sherwood Park Bakery Ltd.		969	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012			left message				
Sherwood Park Dental Care Centre - Children and Family Dentistry		912	Ash St.	Sept. 18, 2012	Oct. 25, 2012			left message				
Sherwood Park Service Car Repair	10	1020	Sherwood Dr.		Oct. 25, 2012			send email				
Sherwood Park Shell		23263	Wye Rd.		Oct. 25, 2012							

WYE Road Master Stakeholder Engagement Database				Phase I	Phase II				Phase III	Phase IV	Phase V	Phase VI
NAME	BUSINESS ADDRESS			Project Kick Off	Questionnaire Distribution	Questionnaire Return/ Participate		Comments	Face-to-Face Meetings	Stakeholder Feedback Sessions	Design Concepts	Final Recommendation
Shig's Flowers		945	Ordze Rd.	Sept. 20, 2012	Oct. 25, 2012						14-May	
Shopper's Drug Mart	5	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012							
Silk Seam Tailors	114	937	Fir St.	Sept. 19, 2012	Oct. 25, 2012	Y	NO					
Smilie's Village Restaurant and Bar		981	Fir St.	Sept. 17, 2012	Oct. 24, 2012	Y	NO				14-May x 2	
Sobey's Nottingham		688	Wye Rd.		Oct. 25, 2012							
Solid Skateboard Shop	49	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012	Y	Y	January 31, 1:00 PM 2004 Sherwood Drive	Mtg #5 - Jan 31			
Spam Equities	103	977	Fir St.		Oct. 24, 2012			John NI, P&R				
	310	975	Fir St.									
	105	977	Fir St.									
	109	977	Fir St.									
Spark Heating & Air Conditioning		115	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012	Y	Y	HOST January 29, 1:00 PM Training Room Sparks Heating and Air Conditioning 115 Ordze Ave	Mtg #2 - Jan 29			
Starbucks		16768	Ash&Wye	Sept. 17, 2012	Oct. 25, 2012							
Strathcona County Museum & Archives		913	Ash St.		Oct. 25, 2012							
Style Lounge Apparel	109	120	Wye Rd.		Oct.24, 2012							
Subway	57	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Tail Blazers, Health Food Store for Pets	133	120	Wye Rd.	Sept. 19, 2012	Oct.24, 2012							
TD Canada Trust		139	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012			phone busy?				
The altView Foundation for Gender Variant and Sexual Minorities	33	99	Wye Rd.		Oct.24, 2012							
The Golf Den		975	Ordze Ave.		Oct. 24, 2012							
The Great Canadian Bagel	101	120	Wye Rd.	Sept. 19, 2012	Oct.24, 2012	Y	NO					
The Style Lounge	105	120	Wye Rd.	Sept. 19, 2012	Oct.24, 2012			send info				
The Water Warehouse	37	99	Wye Rd.	Sept. 17, 2012	Oct. 24, 2012			Back Tues, Jan 15				
Tim Hortons	90	993	Fir St.	Sept. 19, 2012	Oct. 24, 2012							
Tim Hortons- Nottingham	100	664	Wye Rd.		Oct. 25, 2012							
Twisted Yogurt Creations Inc.	138	1020	Sherwood Dr.		Oct. 25, 2012			Left message, will be back Monday				
Urban Barn	21	301	Wye Rd.	Sept. 17, 2012	Oct. 25, 2012							
Urban Organics	130	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012							
VACANT		944	Ordze Rd.									
VACANT		947	Ordze Rd.	Sept. 20, 2012								
VACANT - TriK+G30on Group	106	977	Fir St.									
VACANT - TriKon Group	106	977	Fir St.		Oct. 24, 2012							
Vietnamese Thai Noodle World	80	1020	Sherwood Dr.		Oct. 25, 2012			send info				
Village Park Medical Clinic	110	937	Fir St.	Sept. 19, 2012	Oct. 25, 2012			no answer				
Vince's Auto Care	210	979	Fir St.	Sept. 20, 2012	Oct. 24, 2012							
Wal-Mart		239	Wye Rd.	Sept. 18, 2012	Oct. 25, 2012			Emailed appendices with all materials distributed to stakeholders. Met with representatives from Corporate office				Feb 05 2014
Wendy's Restaurant		198	Ordze Ave.	Sept. 17, 2012	Oct. 24, 2012			called back on Feb 8			14-May x 2	25 June x 2
Western Boots Shoe & Leather Repair	35	1020	Sherwood Dr.	Sept. 18, 2012	Oct. 25, 2012			No time, not interested				
Wild Wing		993	Fir St.	Sept. 19, 2012	Oct. 24, 2012	Y	Y	Call Friday				
Wildrose Party Strathcona-Sherwood Park	11	301	Wye Rd.		Oct. 25, 2012							
Winning Wines Plus/ The BJ's Brew Crew Folks	302	975	Fir St.	Sept. 20, 2012	Oct. 24, 2012			HOST January 31, 1:00 PM2004 Sherwood Drive	Mtg #5 - Jan 31			12-Jun
Yummy Noodle	23	993	Fir St.	Sept. 18, 2012	Oct. 24, 2012			no answer				





## Appendix D.4

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Wye Road Functional Planning Study,  
Public Open House Summary  
(*Al-Terra Engineering*) – 2014



# Public Open House Summary

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## Wye Road Functional Planning Study

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June 2014

Prepared for:





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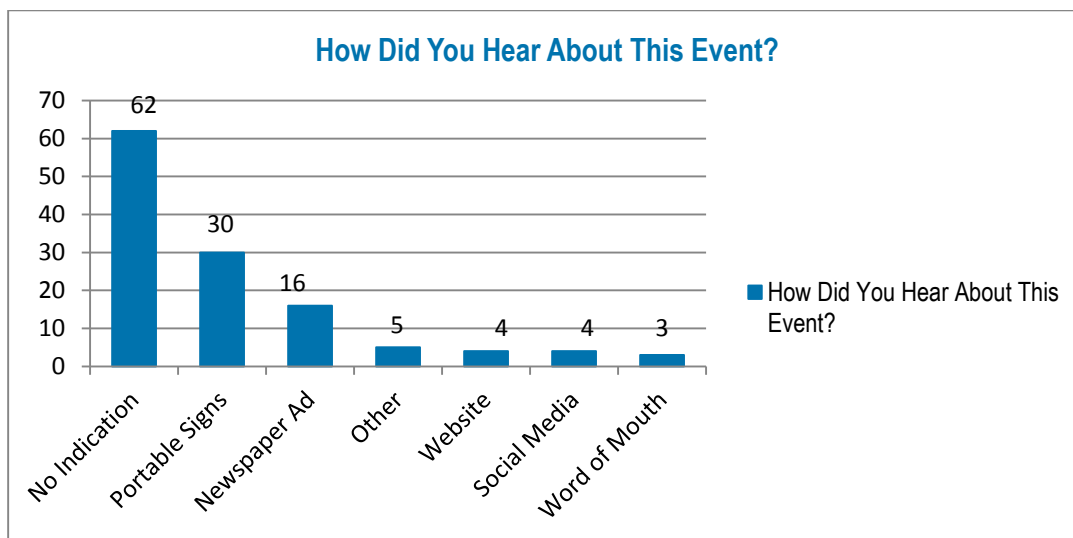
## 1.0 Executive Summary

A public open house for the Wye Road Functional Planning Study was held in the Agora of the Community Centre on Monday June 16, 2014 from 4:00 to 8:00 pm. The purpose of the open house was to present the functional plans prepared by Al-Terra Engineering to the residents of Strathcona County and elicit feedback. The plans showed the proposed roadway improvements from Ordze Road to Highway 21.

Approximately 150 participants attended the open house and 70 comment forms were received. The comment forms asked participants to indicate their likes and dislikes about the proposed plan as well as to provide written comments.

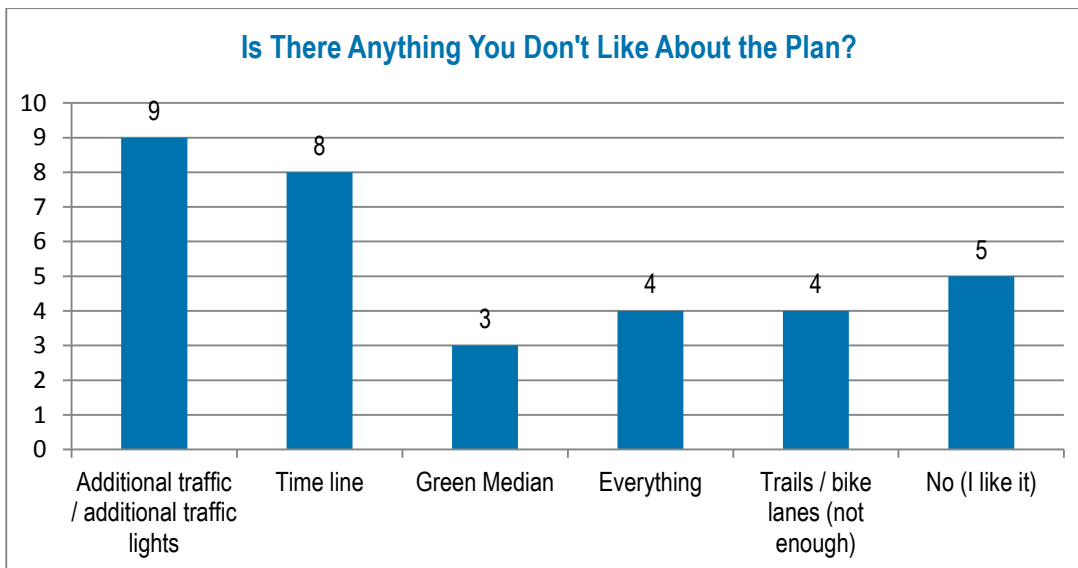
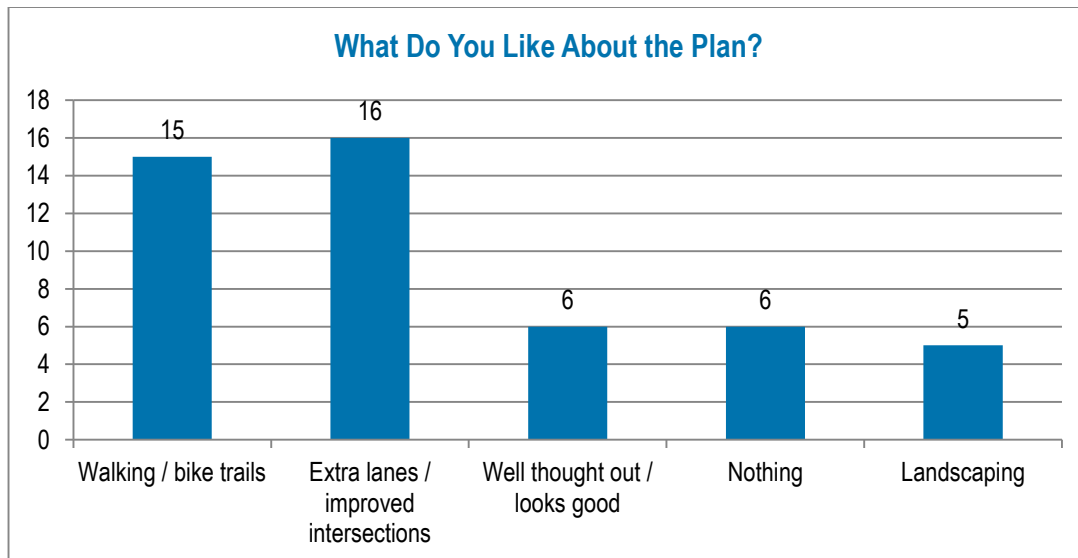
### 1.1 Public Open House Attendance

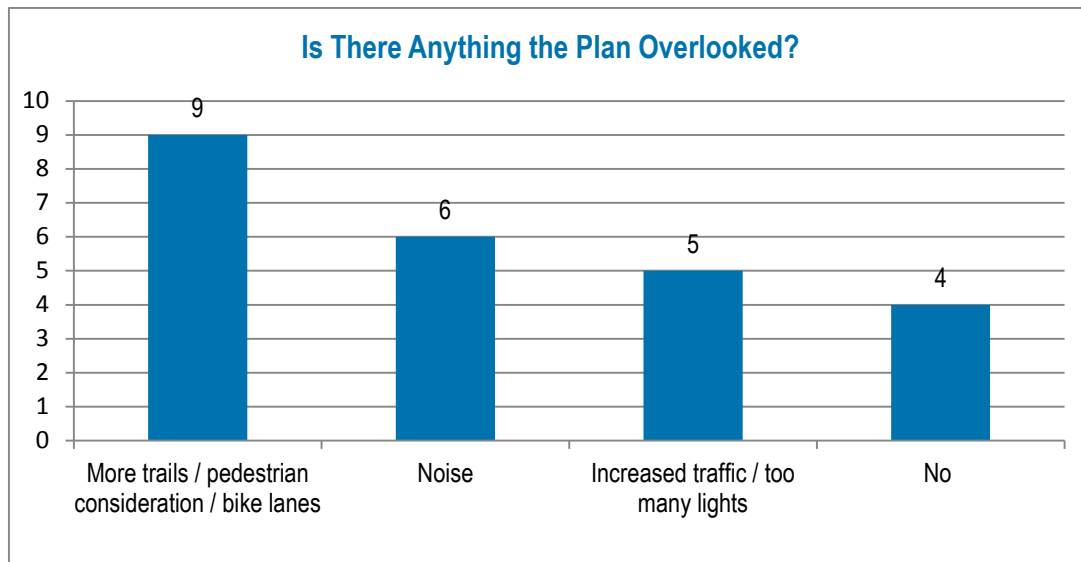
At the Public Open House, 85 sign-in sheets were completed. The attendees were asked the following question:



## 1.2 Common Themes

Some common themes were noted amongst the respondents which are summarized below. The full results from the comment forms are included in Section 2.0





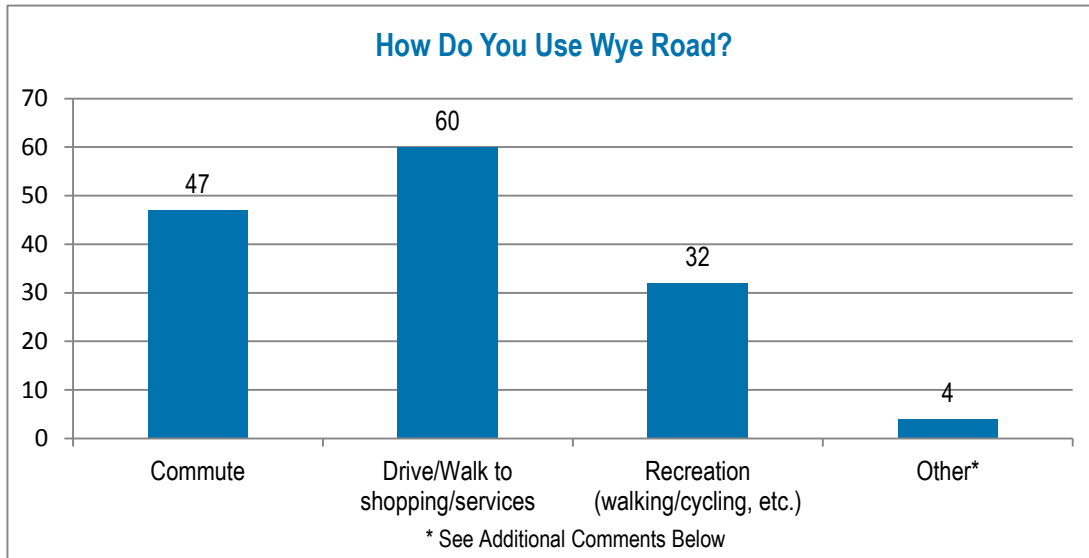
Of the respondents eleven (11) requested they be contacted with further information regarding the project or in response to specific questions.

A copy of the comment form is included in Appendix 'A'.



## 2.0 Wye Road Functional Plan Open House - Evaluation

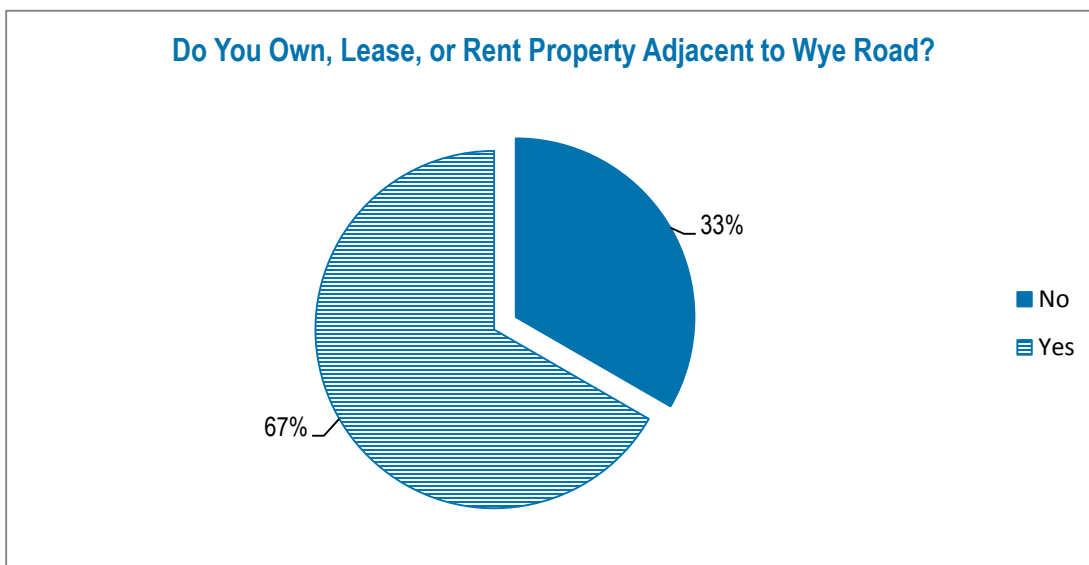
### 2.1 Wye Road Usage

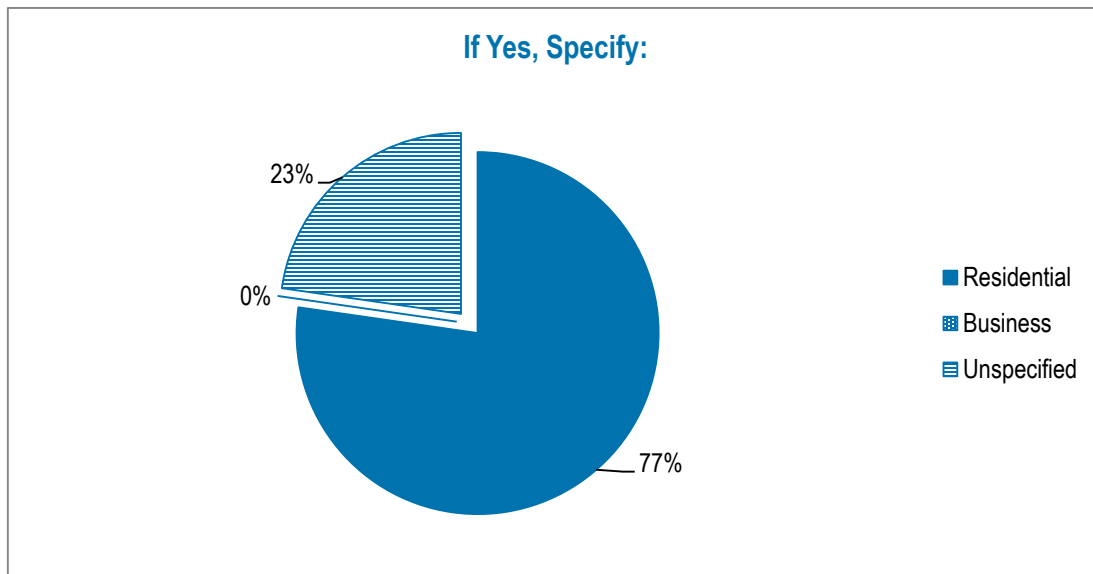


#### Additional Comments

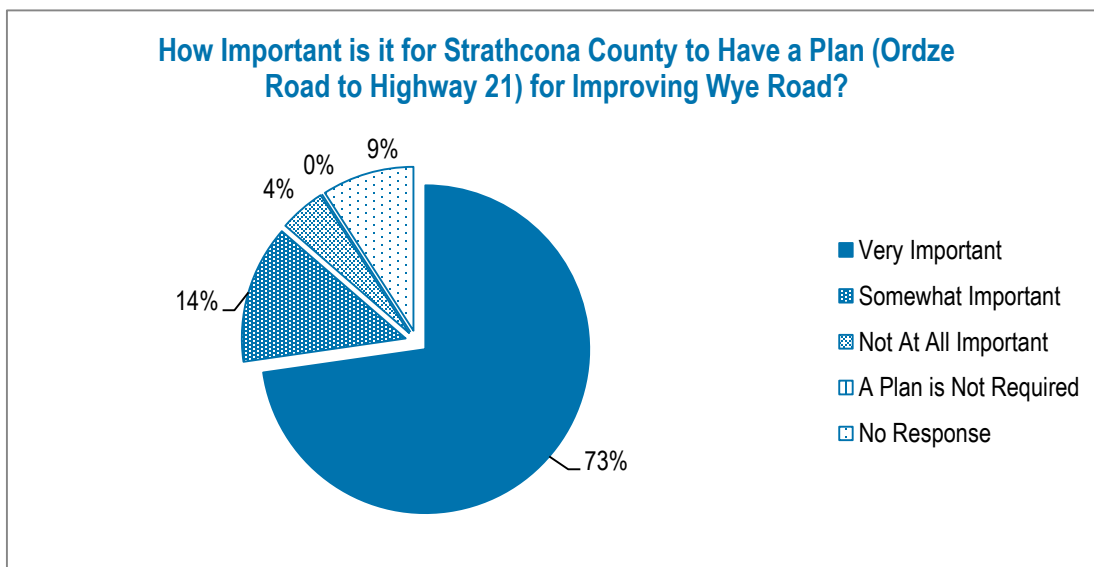
- ♦ East/West arterial incl. access to SP. FWY
- ♦ Would like to travel east safely
- ♦ Resident just south of Wye Road (east side of Campbell town)
- ♦ Adjacent landowner
- ♦ Motorcycle

### 2.2 Wye Road Property Owning, Leasing, or Renting





## 2.3 Importance of Strathcona County Having a Plan for Wye Road



## 2.4 General Responses for the Wye Road Plan

### 2.4.1 What Do You Like About the Plan?

- ◆ Looks good but long overdue. Can it be speeded up?
- ◆ Plan is good. But when are you going to do it?
- ◆ It looks GOOD ON PAPER but please get it done in my lifetime!!!! I am 72 yrs old.
- ◆ It is much better than "highway commercial" which was once proposed and considered!
- ◆ A well develop INITIAL planning document based solely on commercial interests. Public users of this principal E-W arterial deserve more input.

- ◆ Forward thinking.
- ◆ Well, there IS one! It seems well thought out, with consideration of the many stakeholders as well as needs of the community.
- ◆ Appears in depth and long range and accomplishes pedestrian, cycling, etc.
- ◆ Long range planning, sidewalk along south side of road.
- ◆ I like what I see. I think that the plan is well laid out to accommodate both vehicular as well as bike/pedestrian traffic.
- ◆ Like it. However we need to fast track path along Wye from Rona to Cloverbar.
- ◆ TRAILS! All along Wye Road. Trails are a SAFETY issue.
- ◆ Ped/bike trails, some inclination toward 'walkable community'.
- ◆ We like the proposed trail on the south side of Wye Road. Our family uses bikes and trails.
- ◆ Excellent planning for trails and beatification!
- ◆ I like that there is finally going to be a trail for bike all along Wye Rd. This is overdue.
- ◆ That there will be future trails out of the estates East and West along Wye Rd. At least I can walk or bike instead of sinking into a ditch.
- ◆ I like that there are trails planned for walking and biking. I think they need to be built now!! Too dangerous to walk or cycle on Wye Road.
- ◆ A better trail system from Hawthorne to Hwy 21 – this has been discussed before and was part of a planning process 8-10 years ago.
- ◆ The proposed trail on the south side of Wye Road between R.R. 232 and 231.
- ◆ Like the advent of expanded bike/walking paths and improved major thoroughfare traffic lanes/lights.
- ◆ Three lanes to Highway 21 with turning lanes, the walk/bike trail all the way.
- ◆ Extra lanes, turning lanes, walk/trail.
- ◆ More lanes – wider – better intersections for turning.
- ◆ The widening is badly needed.
- ◆ Six lanes would be good but if you add more lights and don't sequence, it is not worth the cost.
- ◆ The intersection at Ash Street is excellent.
- ◆ We like the new configuration of the Walmart intersection and new jughandle intersection.
- ◆ The extension of Ash St. to Ordze Park, lights on corner of 233 and Wye Rd. Gardens.
- ◆ I would like to see vehicles move from Ordze to 21 quickly!
- ◆ It appears that it will improve traffic flow and access.
- ◆ Increased traffic flow.
- ◆ Will minimize congestion during rush hour.
- ◆ It expands Wye Road to three lanes but still incorporates landscaping to make it look nice.
- ◆ The wider medians with trees.
- ◆ Planted medians, inclusion of trails on south side, improved traffic flow.
- ◆ I like the emphasis on plantings. I would encourage the County to do this wherever possible.
- ◆ The fact that the wetland areas in Salisbury Village area will be kept!!
- ◆ Improved intersections, walking/bike trails, safer travelling, easier access, improving landscaping.
- ◆ Traffic on Wye Rd only seems to be a problem on peak hours – widening will increase this problem – divert to Whitemud extension instead.
- ◆ Nothing – Wye is only busy 7-8:30 and 4-5:30.
- ◆ Nothing. We've down this road for schools. Brings lots of traffic on Alder Ave.
- ◆ Nothing. Not designed with thru traffic in mind.

- ◆ Not too much.
- ◆ Nothing.
- ◆ N/A
- ◆ Don't!!!
- ◆ Appreciate opportunity to express concerns if they are given importance. I was told I would be contacted.
- ◆ I like that there is public consultancy. I appreciate the expansion is necessary and that there is a phased plan. I think there is balanced consideration to commercial, residential and traffic safety concerns.
- ◆ We've been down this road for 37 years – doesn't seem to matter what we think!
- ◆ Give us an idea of future changes.
- ◆ Helped to explain what is happening.
- ◆ We live in Estates Court. Would like to see trees along Wye Rd. for noise barriers.
- ◆ Clean up ditches in area.
- ◆ Makes sense to rationalize traffic.

#### 2.4.2 Is There Anything You Don't Like About the Plan?

- ◆ Properties that are close to the affected road changes should have the ability to be included in discussions regarding impact on property and possible rezoning options.
- ◆ Concerns of adjacent landowners has been an uphill battle (S.P. vs Campbell Town Home Owners Assn). Personally I am immediately adjacent to storm water pond, the cemetery – the County access road (between Forbes and Heather Ferguson property). I am the lowest property on Deadman Creek. Water runs north from golf course area east to west natural flow of water and north to south flow from Wye Rd. The connecting culvert to Western flow is too high (plus roadway was repaved and is now higher than before). Water has nowhere to go and part of my property is underwater. Landscaping is destroyed; access to back or other areas of property – obliterated. Buildings would be affected if severe flooding occurred property value is decreased and insurance premiums jeopardized. I have been at this property since the 1970's. Considerable monies and work have been invested in this property. My taxes continue to rise and since the Salisbury village project was initiated, it has all been downhill.
- ◆ Several: remove right out on Sherwood Dr. at Shopper Drug, remove ramp exit S'Bnd at London Drugs, widen Ordze Rd. all the way between S.P. FWY (South Bnd) and Ash. St. extension.
- ◆ I'm confused about the removal of the existing signalization at Walmart on RR233 with the addition of a new signal one block south.
- ◆ The anticipation of additional traffic in unexplained – will Whitemud Ext. not take on an additional load? What future developments east are impacting this plan?
- ◆ All the future traffic.
- ◆ New development will increase school traffic and cut throughs.
- ◆ Not thrilled about extra intersection between Hawthorne and Brentwood.
- ◆ Concerned that the additional traffic lights will impede the flow of traffic. It is important that they are timed properly.
- ◆ I understand there will be additional lights (NO! NO!). When Alder and Hawthorn lights were installed – we were assured that the lights would be sequenced with Sherwood and Brentwood (Linda O) said they would be sequence and if we stopped at Sherwood, Alder



and Hawthorn would always be green until Brentwood – this did not happen. Why is it that Sherwood Park has a policy to “SLOW” traffic down. My understanding of Roadway is to “MOVE” traffic, not slow traffic down. We do a lot of travelling around the world. The light sequencing is the worst I have ever seen in all our travels. It should not take a lot of effort to “WORK TO MOVE Traffic” not slow it down. One other note: green spaces in the middle – really – when salt is used it kills the vegetation and creates additional cost. Pave the middle if you really need a median not green looking for road kill. The bottom line – work hard to “MOVE” traffic now SLOW IT DOWN. Please.

- ◆ Too many traffic lights impediments. Traffic design Wye Rd. to move vehicles not stop vehicles.
- ◆ Concern regarding North Bound traffic turning West on 232 onto Wye Road. Sightlines and timing of signals. Need for expanded trails south bound on 232 from Wye Road (should continue South on 232 to Whitemud extension).
- ◆ No entrance from Highway 21 West into our property – essential as we come from Highway 21 every day. Take space from center median.
- ◆ If stop light added – reduce speed R232 – Hw21. No need for green area in middle of Road, adds costs and maintenance.
- ◆ Greens meridians are too expensive to maintain because of premature death of plants, watering of plants. Use of concrete to guide motorists will cause many problems with snow clearing. There is little point of concrete guides in one direction and not in the other.
- ◆ Meridian with trees. Complete waste of resident’s money to maintain and replace these trees and shrubs.
- ◆ Too much residential and proposed multi-family / apartments, highrises between 232 and Hawthorne, North of cemetery.
- ◆ Please help the Estates be connected to Sherwood Park as quickly as possible (paths).
- ◆ Will really enjoy the new width and bike trails.
- ◆ Extremely disappointed that the plan does not include bike lanes. Bike lanes safely separate vehicular traffic from cyclists.
- ◆ There should be a pedestrian walkway on the north side of Wye Road – from H21 to Sherwood Drive (all the way).
- ◆ Dangerous traffic / pedestrian problem at corner of Brentwood Blvd / Road 232 and Wye Road. More time for pedestrians to cross and better signage.
- ◆ No ‘buffer’ for trails from roadway. All the great plantings are on the residential side of the path.
- ◆ Potential delays in making the trails happen. Someone will be killed on the road as it is right now.
- ◆ The time lines. Would like to see the trail be a priority.
- ◆ The length of time it will take to get the walking trail to Highway 21.
- ◆ The timeline – the trails for walking / biking should be built now!! I feel that the timeline is too long. There is a need now.
- ◆ Wish that some of it was happening faster. The intersection at 232 and Wye Rd needs work. Turning left coming South from 232 is a nightmare.
- ◆ Time frame is too far away.
- ◆ Taking too long to happen.
- ◆ \*Time Line\*
- ◆ Heard about this for years and years. All talk. No firm plans for when it will be completed.
- ◆ Yes there always is and how much will it change before it is done.

- ◆ Mostly everything. Not likely.
- ◆ Everything.
- ◆ Widening – not required – noise abatement not sufficient on South Wye as Estates – Lack of notification of this plan.
- ◆ Are there going to be large cement noise barriers from 232 to 231? Also along 232 from Estates of Per Pk. To Wye. Estates Village needs barriers.
- ◆ Too big, too noisy for all of Nottingham and the Estates.
- ◆ No! I like the plan.
- ◆ Honestly, like all the thoughts that have gone into this presentation.
- ◆ Nothing that comes to mind.
- ◆ Not sure yet.
- ◆ No.
- ◆ Not at this time.
- ◆ Not sure of finances. Will taxes increase?
- ◆ Appears sound but unfortunately driven by expansion and development.
- ◆ No protection to adjacent properties.

### 2.4.3 Is There Anything the Plan Overlooked?

- ◆ On a separate note, need a noise bylaw. Motorcycles revving up at lights are the primary cause of noise pollution – unable to carry on a conversation while sitting in our back yard.
- ◆ Noise.
- ◆ See barriers for noise.
- ◆ Plan does not show any noise attenuation devices / features.
- ◆ Sound / Noise wall for residents that back onto Wye Rd. From Cloverbar Road to Hwy 21.
- ◆ Notification to Estates residents - noise abatement like south side Baseline Rd. Traffic lights increase wait times and don't solve.
- ◆ Too many lights along the way speed will have to be reduced with stopping so often at every intersection.
- ◆ Traffic – input from residents not listened to.
- ◆ The amount of traffic coming out of a high density area (Sherwood Village)
- ◆ Merge lane heading east from Estates to Cloverbar on Wye. Very scary throwing yourself out into lane.
- ◆ Community feelings, traffic.
- ◆ Who is going to pay for this if the additional traffic is because of development? Make them pay not the tax payer.
- ◆ The need for more than four hours of public input. There are 90,000 resident users of these roads in S.C. while the few businesses affected have benefitting from hundreds of hours of consultation at a huge expense.
- ◆ You have not given us a time line.
- ◆ It's a mistake to think that road bikes travelling at 30km/h will mix safely with walkers / joggers / moms with baby strollers on a multi-use path. Instead, road bikes / bike commuters will use the roadway. It's being done now.
- ◆ Bicycle lanes on Wye Rd. for commuters.
- ◆ Confirmation of bike /walking trails south down RR 233.
- ◆ Trail/Walk on south side Wye, clover Bar Rd to Ridgemont.
- ◆ Get trails done!! On South Side Wye.

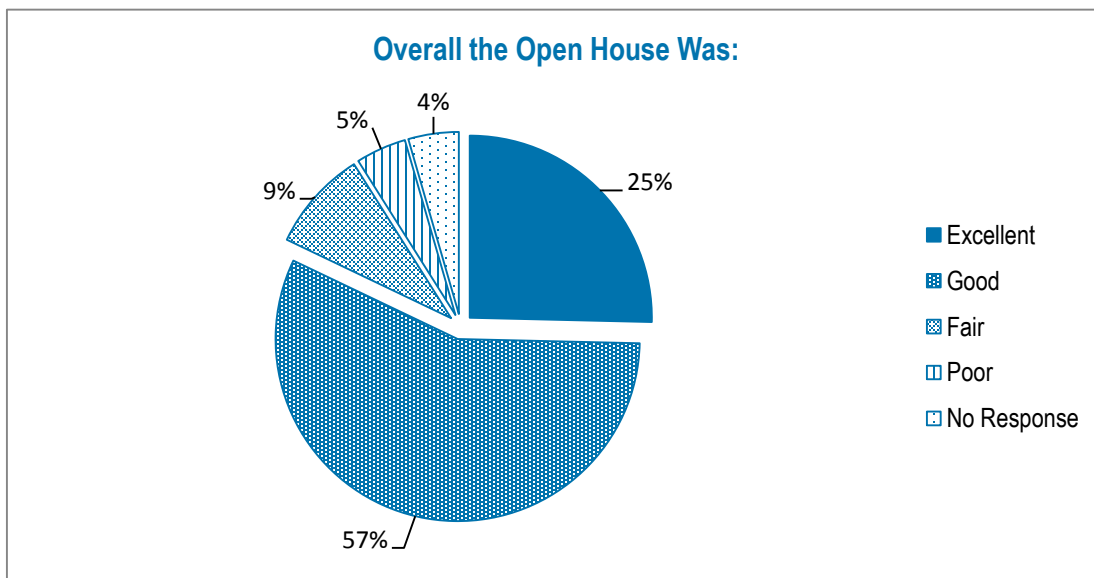
- ♦ Funding to connect the trails built by developers with other trails needs to be a County priority.
- ♦ Heard about a footpath from Rona to CBC tower for years. Do it!
- ♦ Still more consideration for pedestrians perhaps walkways through the large commercial parking lots.
- ♦ I think there needs to be more accommodation for pedestrians crossing at Cloverbar and Wye Road. This is primarily and elementary school transit corridor where children as young as four years old (with pets) pass at peak times.
- ♦ Need signage to clearly signify no U-turn into Estates Court Drive.
- ♦ Good retail, good signage, keep speeds low.
- ♦ Can't think of any now.
- ♦ I think it was well thought out.
- ♦ None that I noticed.
- ♦ Nothing that comes to mind.
- ♦ Not aware of.
- ♦ Distances could have been shown from north side of road to south side.
- ♦ We didn't see any drawings on what will happen to Range Road 233 going south.
- ♦ How about service into Salisbury Village?
- ♦ Makes sense you think about visibility for motorcycles.

#### 2.4.4 Do You Have Any Other Comments About the Plan?

- ♦ Fully involve the public in the consultation process. Currently plan benefits commercial interests.
- ♦ Incorporate the above remarks and other public input and reconvene a public consultation and review. Delete proposed traffic control intersection west of Brentwood at Salisbury Village. Construct service roads, add scale.
- ♦ Concerned about current traffic danger west of Wye to Hawthorne – former turning lane changed to through lane – traffic turning west off Hawthorne have poor vision of oncoming traffic and would turn right when oncoming traffic goes into lanes very fast moving traffic potential serious collisions.
- ♦ Looking forward to removing / reworking the intersections immediately north and south of Wye Road on Sherwood Drive. Traffic volumes high and with high accident rates.
- ♦ When northbound from Wye Road on Clover Bar Road – all merge lanes should be this long (very good for safe merging).
- ♦ More traffic – more signal lights not synchronized. Traffic on Alder is BAD NOW. Will be a nightmare then.
- ♦ Please have the traffic lights at 231 and Brentwood working so that the green turning light works at ALL times.
- ♦ Intersection lights at Ridgmont Way should be sooner than 11 years.
- ♦ Would like a service road versus street light at every corner. Not thinking in the future.
- ♦ Growth and traffic are somewhat inevitable, but the County does not need to encourage higher population.
- ♦ An improvement over what you had planned a few years ago with intersections and walkways.
- ♦ Wye Road between Brentwood and 21 looks like crap. Fix it.
- ♦ The road needs to be improved now!

- ◆ It will be nice to see more trail and infrastructure updating on this side of town.
- ◆ A very high priority is a pathway on Wye Road between Brentwood and RR 231 (Cloverbar Road).
- ◆ Planners and communities that are forward-thinking and visionary are building bike lanes. These are different than multi-use paths. Now is the time to integrate and plan for this.
- ◆ Keep as much green space as possible please; keep adding plants, etc.
- ◆ We want a fence to give use some barrier from noise and traffic into our easement.
- ◆ My main concern is the cars and motorcycles with the loud mufflers. Will there be more enforcement and are you looking at a NOISE bylaw?
- ◆ We are hoping that the concrete wall on the north side of Wye between Brentwood and the CBC Tower will eventually be covered with some dense foliage to absorb road noise.
- ◆ I would like to see some things that would minimize / reduce the traffic noise.
- ◆ We bought in Sherwood for a reason – that reason is being taken away.
- ◆ Too vague as to time lines.
- ◆ Lots of talk not sure action will follow.
- ◆ There should have been some folks from planning and development to explain what is being proposed along Wye Rd.
- ◆ Go for it!
- ◆ Good luck!
- ◆ Let's get rolling – time to upgrade Wye Rd.

## 2.5 Overall Opinion of the Open House

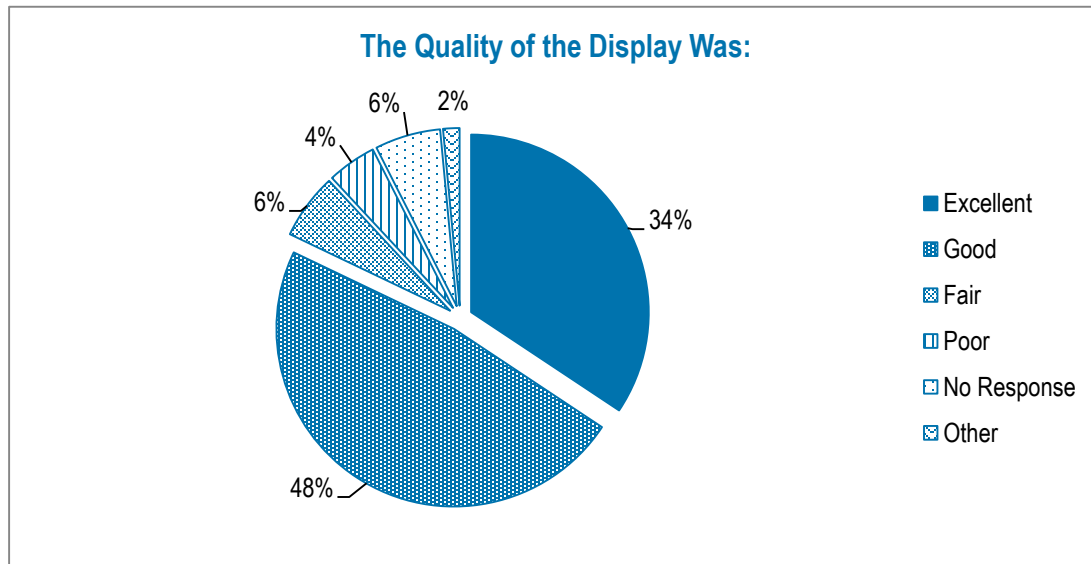


### Additional Comments

- ◆ Poorly advertised to residents.



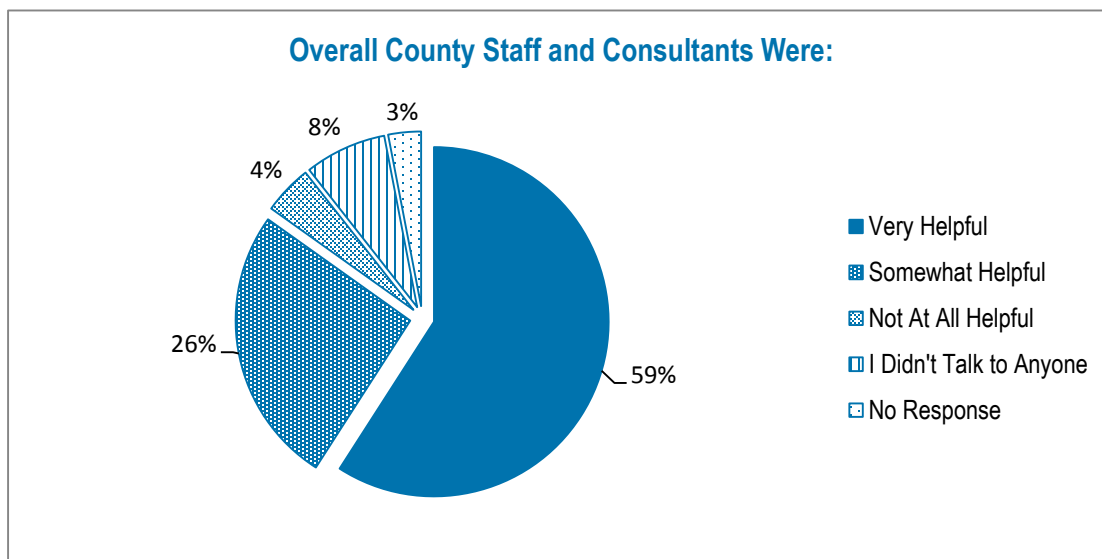
## 2.6 Overall Quality of the Display



### Additional Comment

- ♦ Too complicated.

## 2.7 Overall County Staff



### Additional Comments

- ♦ One evening only – why isn't this display open for more residents to view?
- ♦ Feel don't want constructive feedback so they really don't want to talk.
- ♦ Not sure whether they can really happen to solve the concerns expressed. I hope so!

### 3.0 Additional Feedback

The following additional feedback was modified to conceal the writers' identities.

Writer One:

#### Concerns with Wye Road Expansion

1. If memory serves me correctly, this proposed expansion is in complete violation of an agreement made with the County 30 years ago, where again, only going by memory, the County agreed that Wye Road would remain at 4 lanes, Heavy Truck traffic would be diverted to Hwy. 14 (now Anthony Henday) and a sound barrier would be erected along all adjacent properties.
2. Personally I'm not aware of any proposed developments in close proximity to Wye Road that would warrant this expansion.
3. Any future developments, North and South, will create additional traffic on Hwys. 16, 21, 14, Whitemud Extension and Anthony Henday.
  - Therefore, any hwy. expansion should be the widening of Whitemud Extension from Anthony Henday to Hwy. 21 to 4 lanes.
4. Widening Wye Road further will encourage additional heavy truck traffic to Hwy 21, increasing the already unacceptable noise and exhaust pollution.
5. It would appear there was no consideration given to the backlog of southbound traffic on Brentwood that will occur while attempting to turn west onto Wye Road during morning rush hour with the removal of the acceleration lane.
  - How many other intersections will be similarly affected?
  - How many accidents will occur until people are aware of this change?
  - One major accident will severely disrupt morning rush hour.
  - Alder Ave., a residential street, will then end up becoming an alternate route.
6. I have spoken with Sgt. C. Narbonne of RCMP Traffic Services (780-449-0129) and was advised that they have not been asked to voice any concerns regarding traffic enforcement.
7. Traffic enforcement on certain sections of Wye Road must be difficult at present; this will only become more difficult with expansion.
8. At present there is an 8" culvert under the concrete wall to drain my property in the spring; what will happen with that?
9. Maybe not directly related to Wye Road Expansion but, in 2006 (35 years after construction), my basement flooded, suggesting a rising water table. This necessitated gutting and rebuilding, including the installation of a sump pump, which has since been functional every spring. What provisions are being made to ensure the water table remains as is or is lowered?
10. For the past 35 years I have been exposed to excessive noise, dust and exhaust pollution from inadequate protection, this is bound to increase and there does not appear to be any provision in the plan to reconstruct satisfactory attenuation barriers.
11. On the good days/nights and dry roads I'm also subjected to the noise of dragsters and speeders who seem to know when traffic enforcement is not around; this also will only increase.
12. My property value has already depreciated from this unpleasant environment, only time will tell how much more.

Writer Two:

Having reviewed the analytical portion (pp 1-41) of the above report, I offer my initial primary observations and concerns, as follows:

A. This is a well developed undertaking, inclusive of a wide range of views, opinions, needs, wants and expectations of the commercial interests involved in its preparation. Stantec consultants undertook an orderly and functional approach to garnering input and feedback from the business community in the area affected, and generated several plausible Wye Road (and adjacent roadways) redevelopment options.

B. The most evident shortcoming of the above report lies in the fact that it DOES NOT incorporate any semblance of "public" input. As clearly stated throughout this report, "stakeholder" input and recommendations (i.e. affected business community) serve to generate the Phase VI (June 2013) Final Recommendations, as presented on page 41. In and of itself, this is a reasonable outcome of the Wye Road Functional Planning Study.

HOWEVER, when I read, at the top of page 7 that "The recommended (i.e. stakeholder endorsed) plan WILL be incorporated into the overall FPS for Wye Road and presented to County Council for consideration AND APPROVAL", I become concerned that public review and input will NOT be undertaken. (*4 hours may not be adequate to address competing issues*)

C. Another evident shortcoming of this report is its complete lack of discussion of the "non-business"-related traffic management challenges associated with any Functional Planning Study (FPS) for Wye Road and adjacent roadways. I suspect that there are far more users of the routes under study, on a daily basis, than any estimate of those intending to access one or more of the businesses affected. These are primarily the residents of Sherwood Park and Strathcona County who drive along, through and by the affected businesses on a daily (or, twice daily?) basis. For example a resident commuter might pass along or through this area on the way to and from their place of work many more times than they might while attending to their weekend shopping experiences at affected businesses. Additionally, this study makes no mention of the implications of the outcome of the soon-to-be-completed Henday (Hwy. 216) improvements. I suspect that traffic patterns and volumes we have known to prevail over the past few years will be significantly altered once construction is completed. This aspect of the Wye Road FPS will require further discussion and analysis from both business and public perspectives.

D. Finally, in an emerging climate of transparency and openness of governance and civic administration, I urge you and County Council to take the above report into consideration simply as a foundation document and an excellent initial point of discussion. This approach should set the stage for a broader level of public discussion, input and amendment, as appropriate, prior to Council endorsement and approval of a Wye Road (and affected roadways) redevelopment plan. In my 40+ years as a resident of Sherwood Park, I have witnessed far too many examples of "business-driven" development initiatives, devoid of public commentary, that have created the sorts of traffic management challenges we currently face in this community. Let's not perpetuate this sort of short-sighted planning and development in the course of mitigating prior shortcomings.

PS. I have also noted (within the document) some personal points of contention with a couple of the roadway design recommendations (shortcomings?), but will hold onto these until such time as public dialogue is undertaken.

Writer Three:

**March 6, 2014**

Strathcona County  
2001 Sherwood Drive  
Sherwood Park, AB  
T8A 3W7  
Attn: Transportation & Agriculture

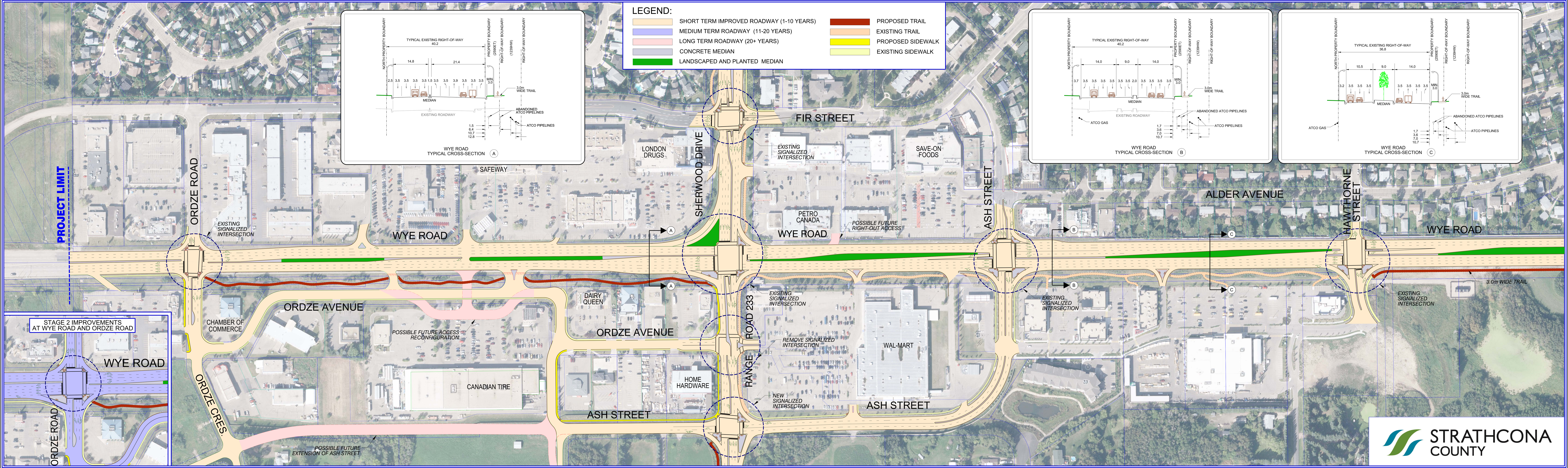
Dear Sir or Madam :

I would like to bring to your attention, the existence of a very dangerous traffic problem, at the corner of Wye Road and Brentwood Blvd/Range Road 232.

During the past few years the volume of traffic has increased significantly at this intersection. It is especially hazardous for pedestrians, cyclists and the handicapped who attempt to cross the road, in either a northerly or southerly direction. Personally, on several occasions, I have almost been struck by inattentive drivers who are making a turn on to Wye Road. It should be noted that the many seniors who live in condominiums in Estates Court (situated within approximately 100 meters of Wye Road) are frequent users of this intersection.

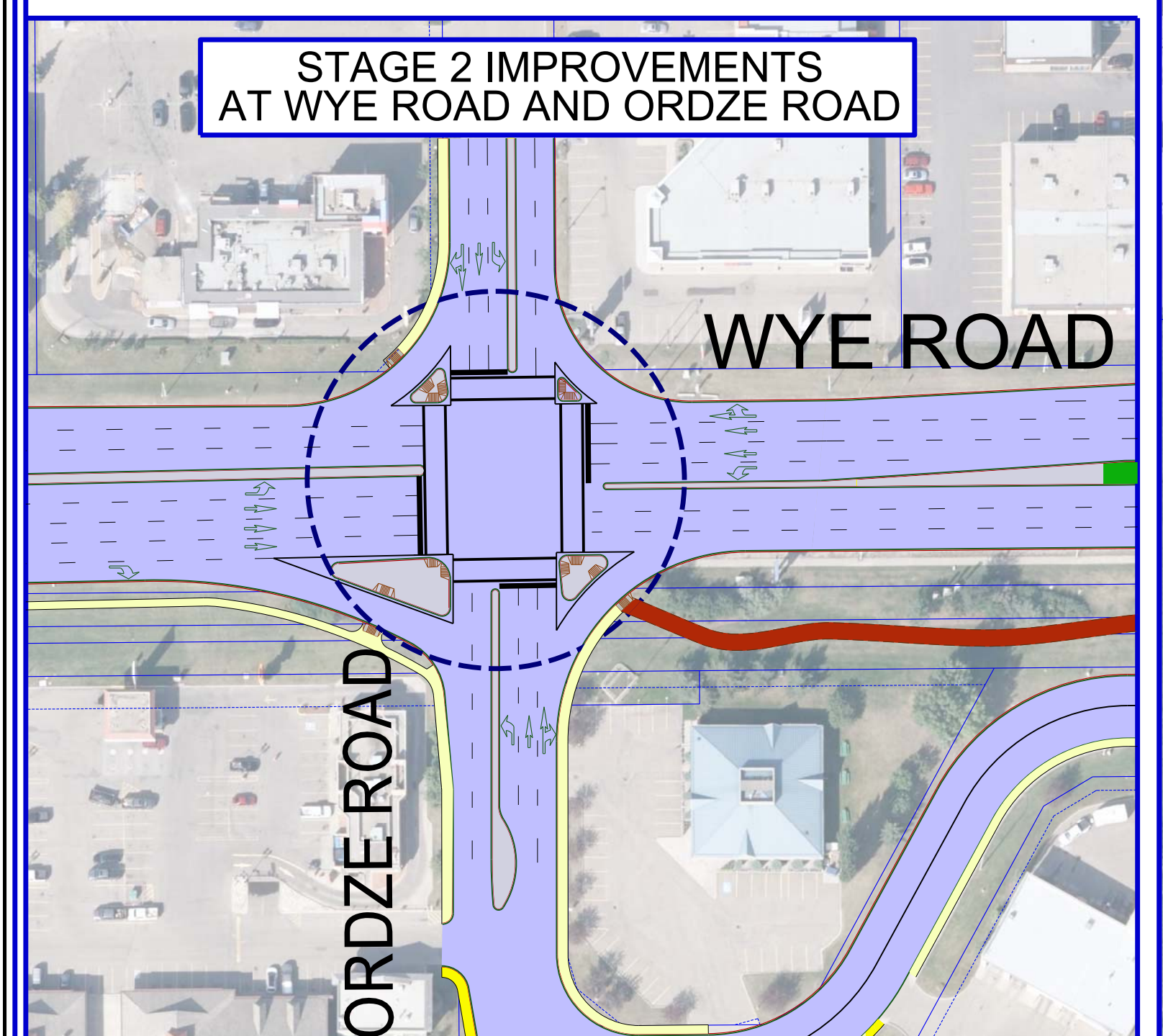
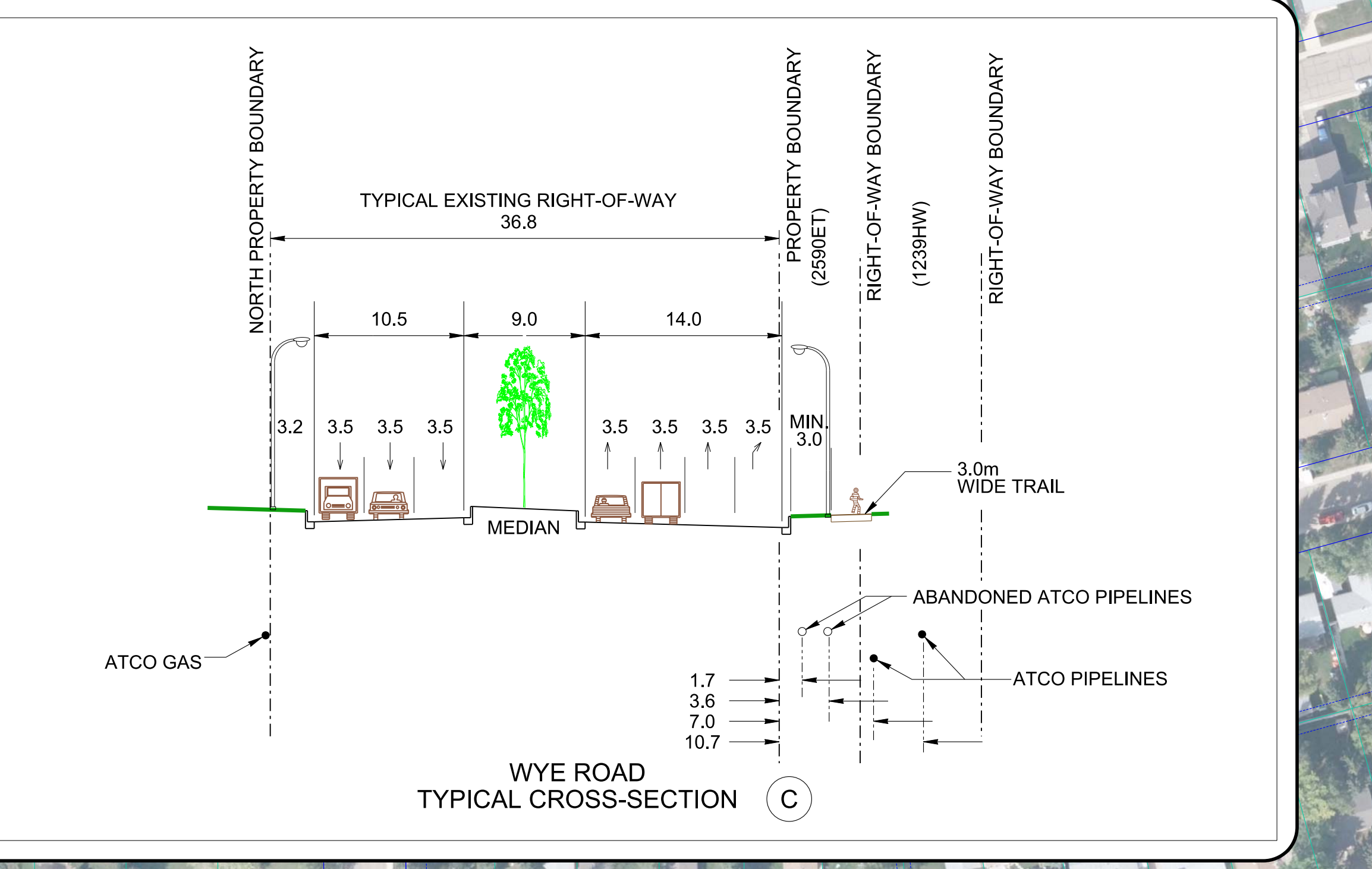
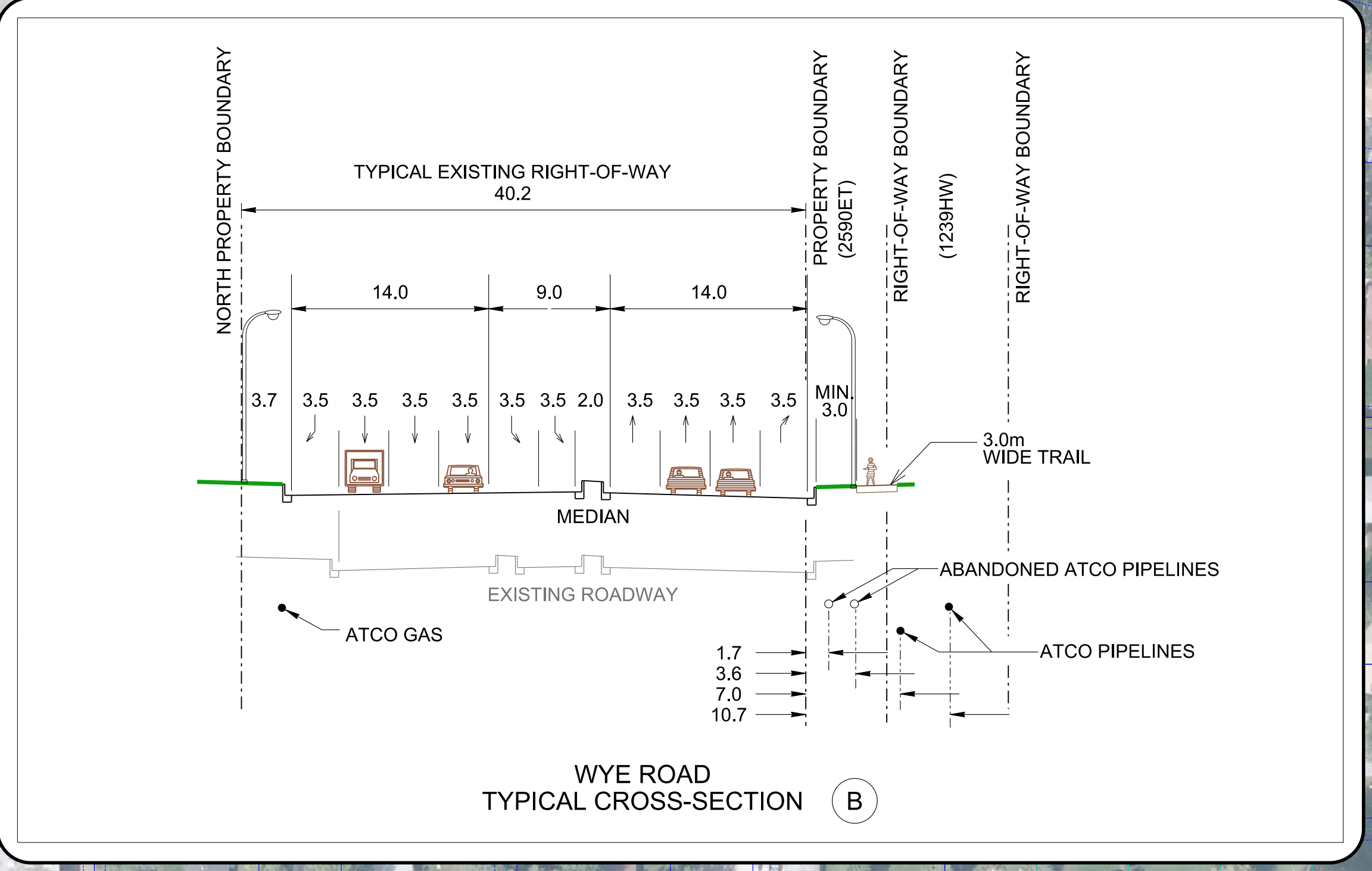
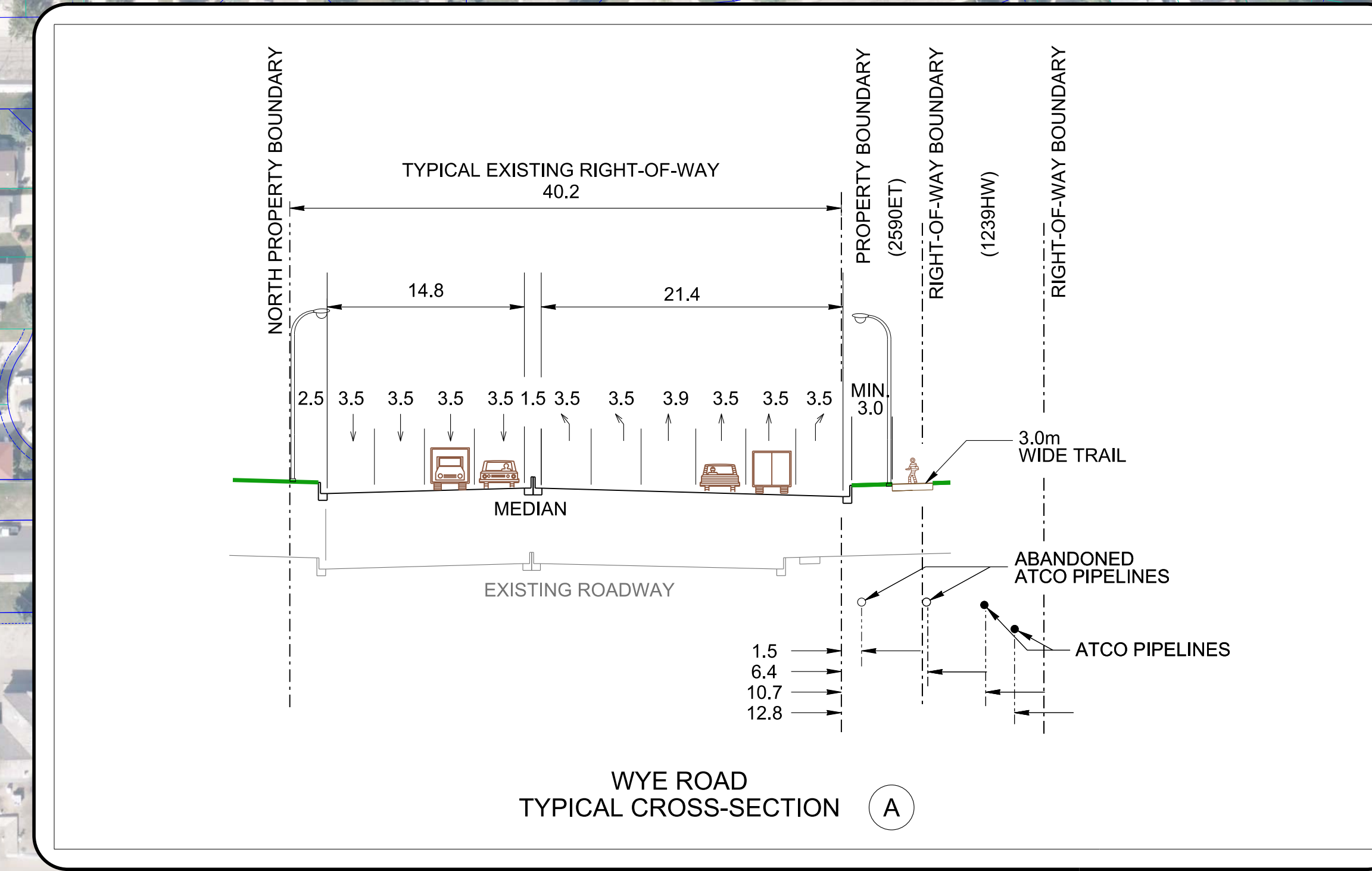
It certainly appears that immediate action should be taken to rectify this very perilous situation before a serious accident occurs. I look forward to hearing from you, as to the steps that will be taken to resolve this matter.





**LEGEND:**

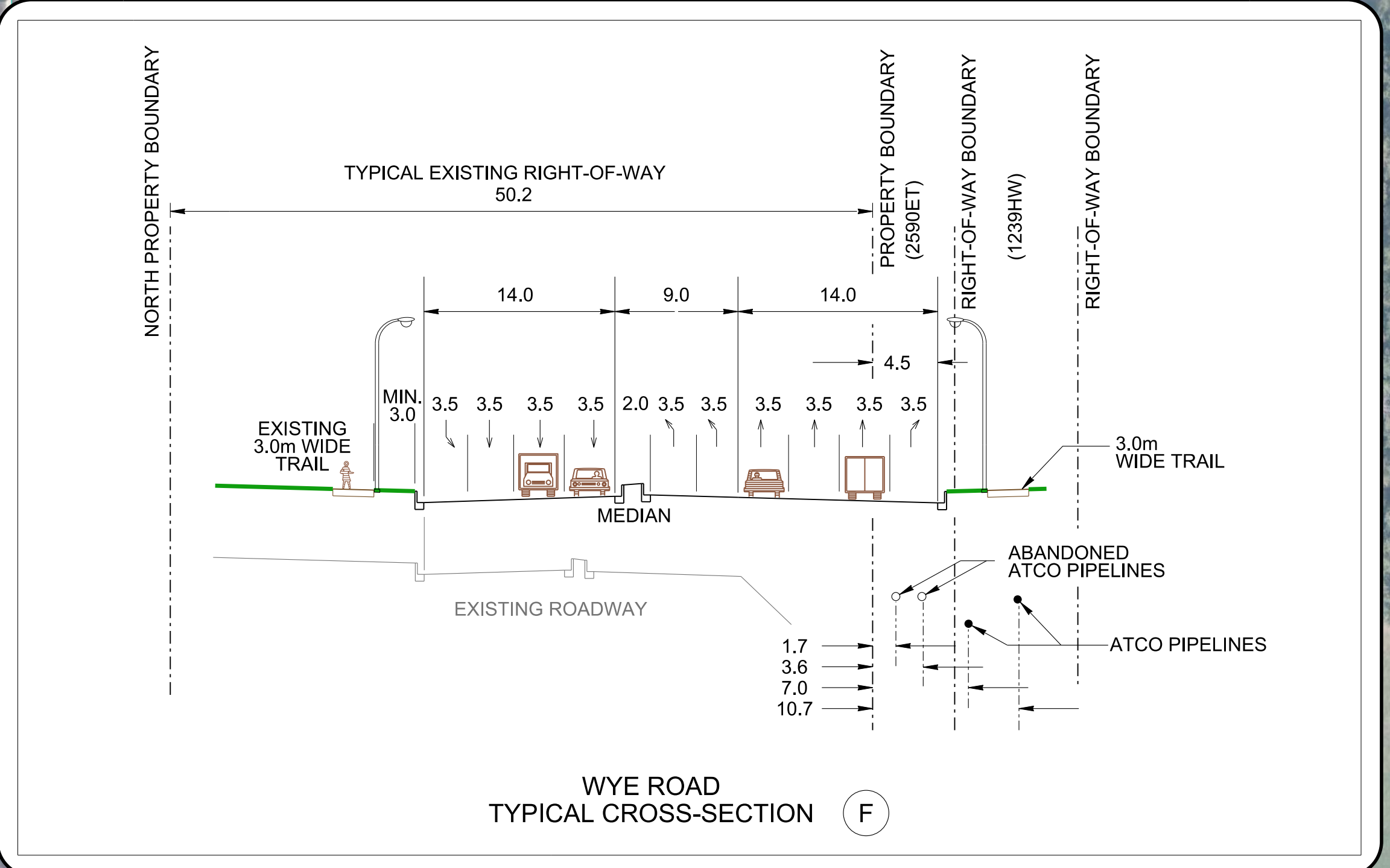
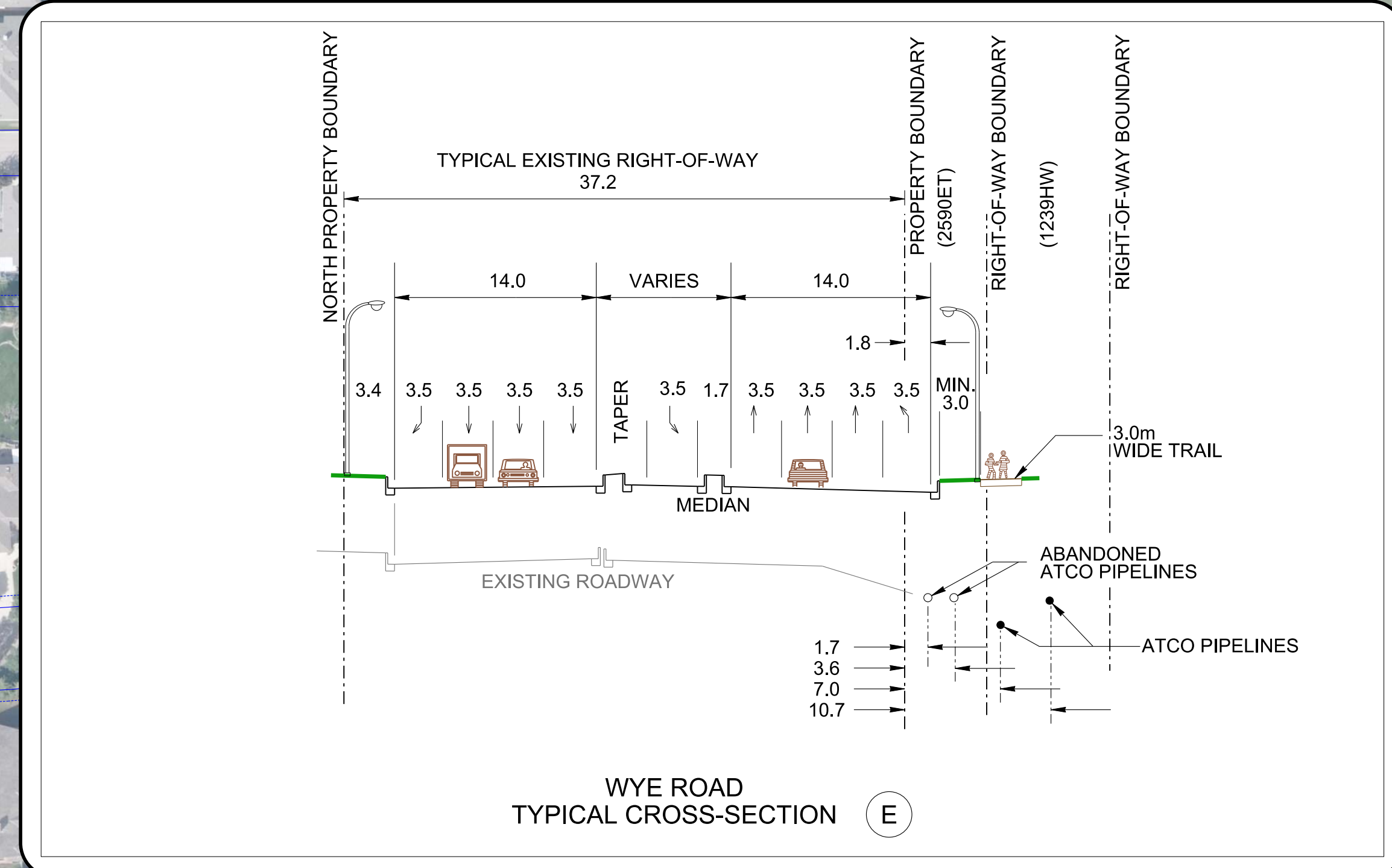
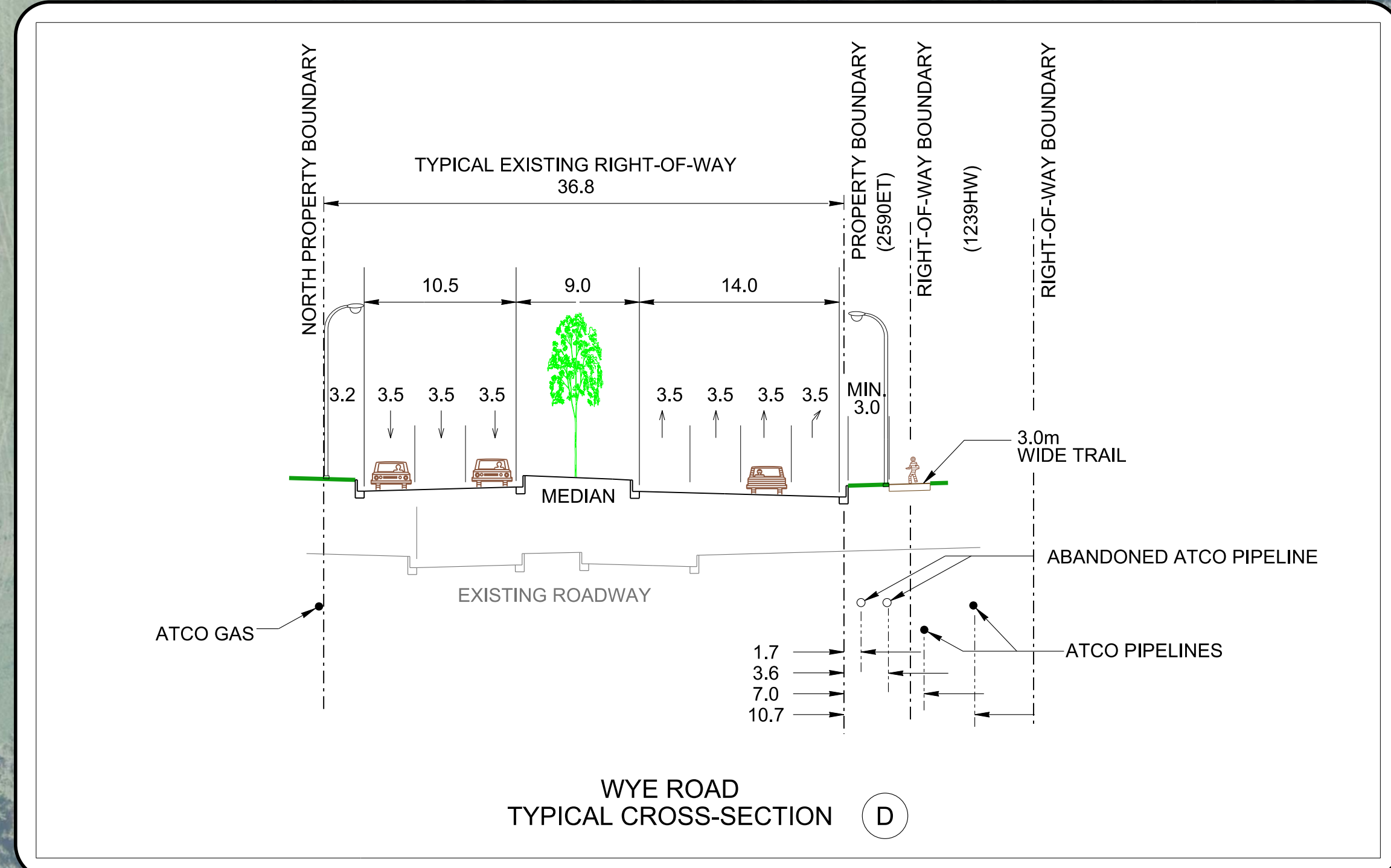
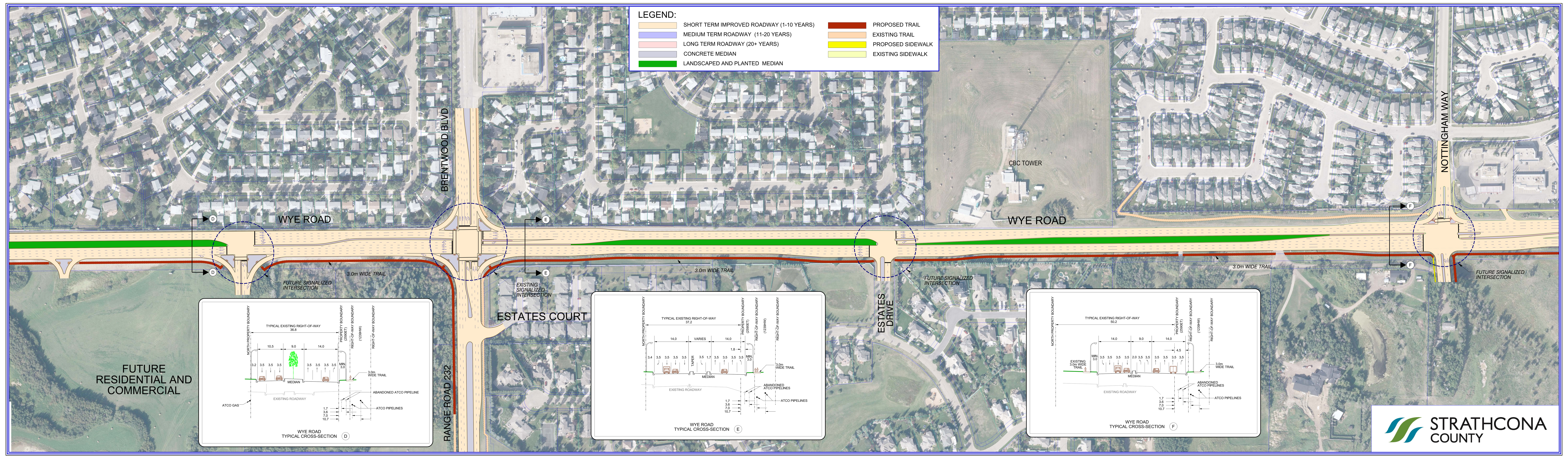
	SHORT TERM IMPROVED ROADWAY (1-10 YEARS)		PROPOSED TRAIL
	MEDIUM TERM ROADWAY (11-20 YEARS)		EXISTING TRAIL
	LONG TERM ROADWAY (20+ YEARS)		PROPOSED SIDEWALK
	CONCRETE MEDIAN		EXISTING SIDEWALK
	LANDSCAPED AND PLANTED MEDIAN		





**LEGEND:**

	SHORT TERM IMPROVED ROADWAY (1-10 YEARS)		PROPOSED TRAIL
	MEDIUM TERM ROADWAY (11-20 YEARS)		EXISTING TRAIL
	LONG TERM ROADWAY (20+ YEARS)		PROPOSED SIDEWALK
	CONCRETE MEDIAN		EXISTING SIDEWALK
	LANDSCAPED AND PLANTED MEDIAN		





**LEGEND:**

	SHORT TERM IMPROVED ROADWAY (1-10 YEARS)		PROPOSED TRAIL
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	LONG TERM ROADWAY (20+ YEARS)		PROPOSED SIDEWALK
	CONCRETE MEDIAN		EXISTING SIDEWALK
	LANDSCAPED AND PLANTED MEDIAN		

