

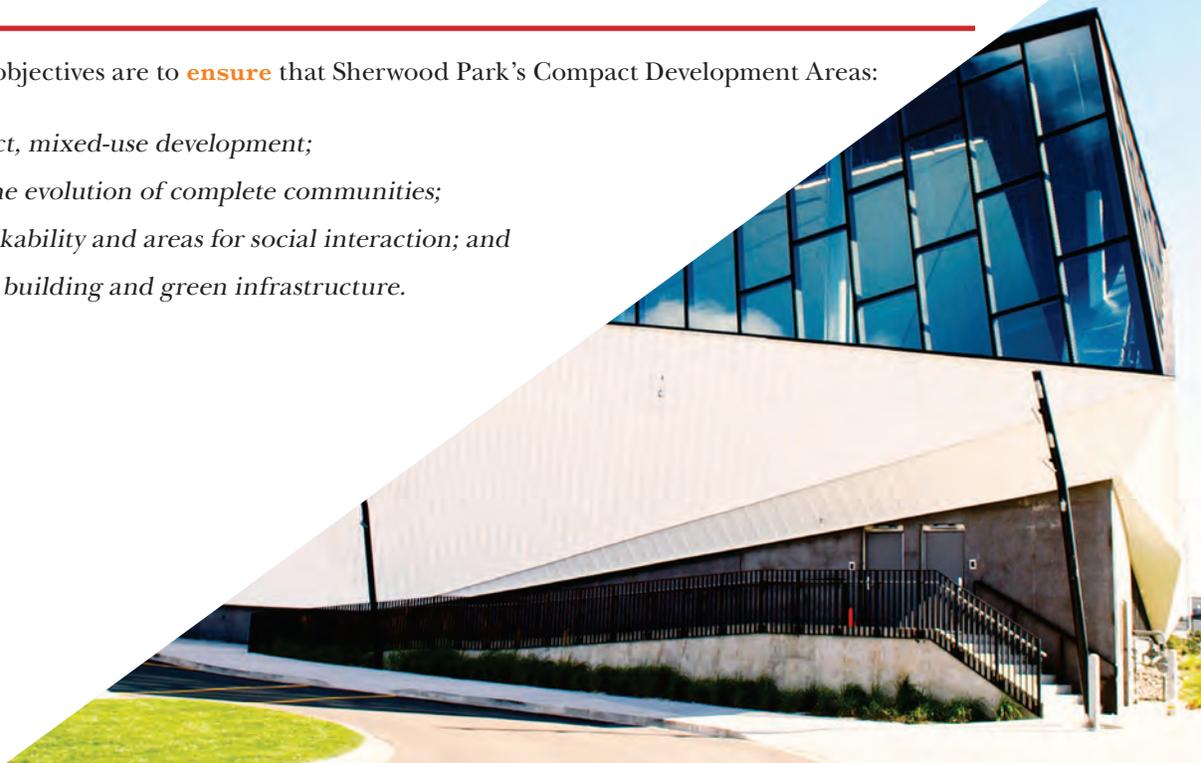
4.3.3 COMPACT DEVELOPMENT POLICY AREA

This policy area comprises existing commercial, residential and open space areas identified on Map 3: Urban Service Area - Sherwood Park predominantly located along Sherwood Drive, Emerald Drive and Wye Road. Presently this area contains major public services such as Emerald Hills Regional Park and the Strathcona Community Hospital. This area also includes areas such as the Palisades Urban Village, Centennial Park and Centennial Business Park, Emerald Hills Shopping Centre, Emerald Hills Urban Village, Salisbury Village and Wye Road Commercial sites. The higher density of existing residential and community housing developments and their nearness to existing commercial and major public services creates an opportunity for this area to promote redevelopment and infill in the form of mixed-use development within existing commercial. As more compact development occurs, these areas can work towards Transit Oriented Development given that these areas are located along major transportation corridors. As intensification opportunities arise, the buildings as well as surrounding landscape shall be planned and built to meet the objective and policies identified below. Future development should be compact to encourage walking, social interaction and transit use. There may be future potential for a priority transit corridor and on street, at grade, transit transfer facility that will service the Emerald Hills area as the Strathcona Community Hospital, employment and amenities make the area a destination for Sherwood Park.

Objectives

Strathcona County's objectives are to **ensure** that Sherwood Park's Compact Development Areas:

1. *Contain compact, mixed-use development;*
2. *Contribute to the evolution of complete communities;*
3. *Incorporate walkability and areas for social interaction; and*
4. *Integrate green building and green infrastructure.*



Policies Strathcona County Will



General

Ensure the evolution of complete communities by **requiring**:

1. Area Redevelopment Plans to incorporate and provide direction on all of the following:
 - a. character defining elements;
 - b. intensification;
 - c. transit oriented development;
 - d. high and medium density residential;
 - e. schools;
 - f. community and seniors housing;
 - g. commercial including community commercial, business commercial and mixed-use development;
 - h. urban agriculture;
 - i. open space, public agriculture and pedestrian infrastructure;
 - j. local and major community services;
 - k. multi-modal transportation including public transportation;
 - l. maintenance and enhancement of active transportation infrastructure;
 - m. crime prevention through environmental design;
 - n. infrastructure including low impact development for cold weather climates; and
 - o. design guidelines.

Promote the evolution of complete communities by **encouraging**:

2. a mixture of uses including major public services, commercial, residential, schools and local community services.

3. redevelopment and infill within the Compact Development Policy Area.
4. opportunities for all ages, incomes and abilities within this policy area.

Support the evolution of complete communities by **considering**:

5. intensification through apartment style medium and high density residential as well as mixed-use development where the development is in conformance with the applicable Area Redevelopment Plan or:
 - a. is located within an existing site containing services;
 - b. improves opportunities for people to use transit;
 - c. incorporates pedestrian infrastructure;
 - d. respects the character defining elements found within the existing neighbourhood;
 - e. contains massing of a compatible scale;
 - f. contains infrastructure which meets the requirements of municipal and provincial standards and regulations;
 - g. considers and values community input; and
 - h. is consistent with an applicable Area Structure Plan.



Residential

Ensure compact, mixed-use development by **requiring**:

6. residential to be within close proximity to mixed-use development, commercial, local community services, schools or major community services.

Promote compact, mixed-use development by **encouraging**:

7. intensification resulting in one or more of the following in conformance with an Area Redevelopment Plan or Area Structure Plan:
 - a. high density residential;
 - b. apartment style medium density residential;
 - c. mixed-use development;
 - d. a range of residential tenures;
 - e. community and seniors housing; or
 - f. innovation in residential design with distinct character.
8. new or redeveloping high or medium density residential and mixed-use buildings to front onto streets with a zero setback at the ground floor level.

Ensure the evolution of complete communities by **requiring**:

9. the recognition of the contribution existing residential provides to mixed-use development in Area Redevelopment Plans and Area Structure Plans.

Support the evolution of complete communities by **considering**:

10. inclusionary housing within Area Redevelopment Plans or Area Structure Plans where in close proximity to existing or proposed services.



Commercial

Ensure the evolution of complete communities by **requiring**:

11. a high standard of architectural appearance, design and landscaping along major streets that serves to enhance the visual form and character of development.

WHAT IS A COMPLETE COMMUNITY?



Accessibility

Provides convenient access to housing, employment and services using multi-modal transportation.



Housing Diversity

Includes a wide range of housing types and tenures for all ages, incomes and abilities.



Daily Needs

Day-to-day needs can be met within your community.

Promote compact, mixed-use development by **encouraging**:

12. commercial sites to include mixed-use development including, residential, local community services, high schools or major community services.
13. intensification of commercial sites through the infill of surface parking lots and redevelopment into higher density, multi-story commercial or mixed-use buildings.

Promote the evolution of complete communities by **encouraging**:

14. new and existing business commercial such as offices and professional services that provide employment close to residential.

Promote the incorporation of walkability and areas for social interaction by **encouraging**:

15. community commercial uses to be built in close proximity to residential.
16. that new or redeveloping commercial sites incorporate pedestrian-oriented design by:
 - a. including buildings that front onto a street with a zero setback at the ground floor level;
 - b. creating the appearance of regularly

spaced frontages;

- c. including storefront windows and street fronting entrances; and
- d. including outdoor seating/patios.



Major Public Services

Promote compact, mixed-use development by **encouraging**:

17. major community services to locate within mixed use development or in close proximity to commercial, residential or local community services.

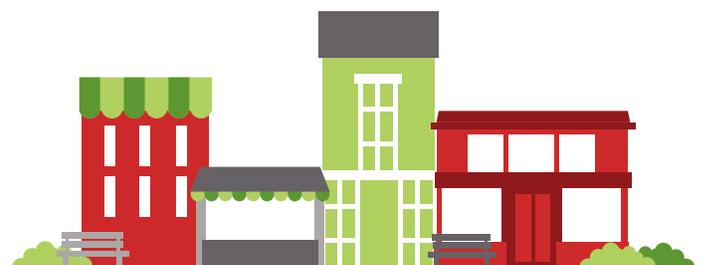
Ensure the evolution of complete communities by **requiring**:

18. major public services including major community services.
19. that public indoor recreation facilities to also contain public outdoor recreation amenities.
20. the continued operation, improvement and assessment of the Ordze Transit Terminal and provide for required modifications.

Promote the evolution of complete communities by **encouraging**:

21. a variety of public and private major community services.

There is an opportunity to promote redevelopment and infill in the form of mixed-use development within commercial areas.



22. that major community services:
- maintain a central location within a community or the Urban Service Area;
 - provide a range of services;
 - locate along new, existing or planned transit corridors;
 - be connected to active transportation infrastructure;
 - implement traffic calming at adjacent crossings;
 - be accessible from all sides;
 - contain or be adjacent to an outdoor gathering space;
 - be highly visible;
 - contain ample bike parking; and
 - consider potential expansion areas.
23. that major community services provide amenities for all ages, incomes and abilities.



Schools

Ensure compact, mixed-use development by **requiring**:

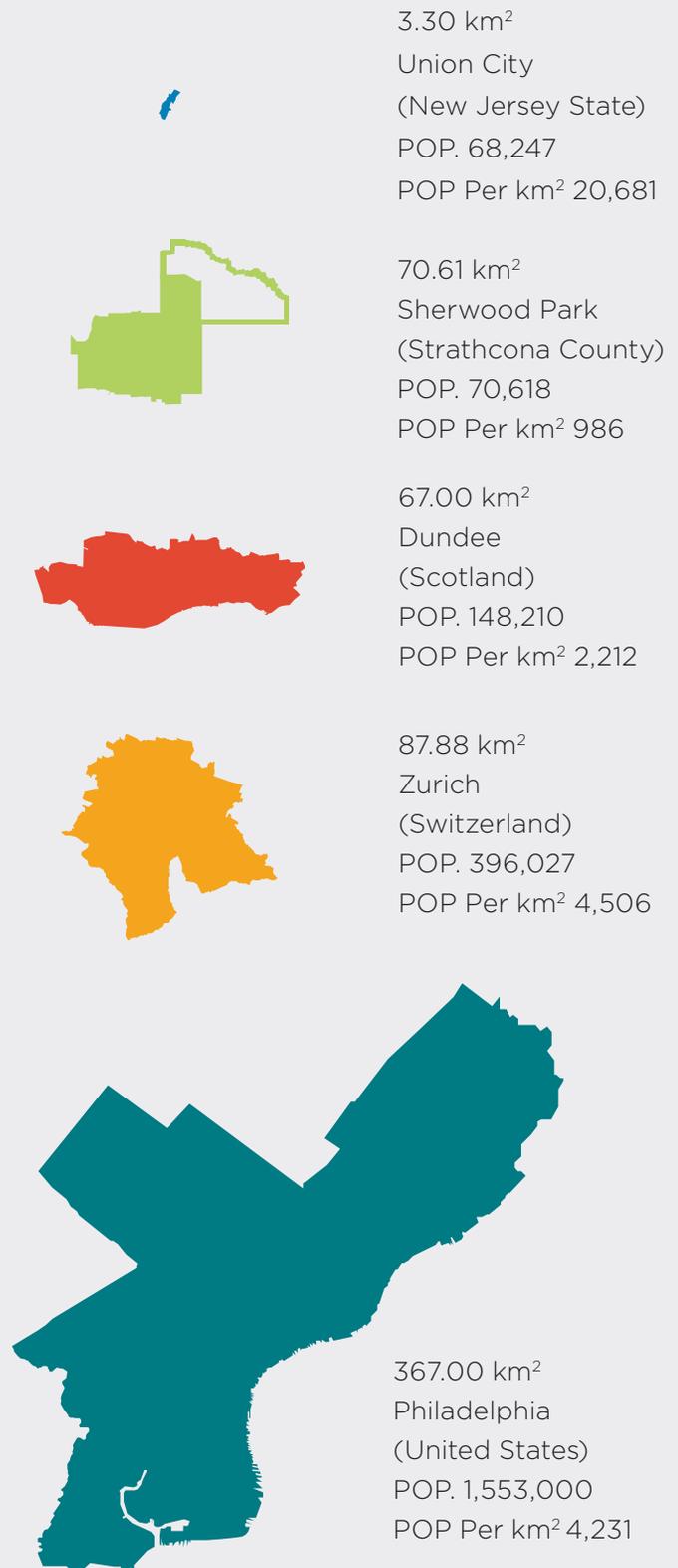
- school sites to locate in close proximity to residential.
- school sites to locate in close proximity to local community services or major community services.

Promote the evolution of complete communities

by **encouraging**:

- high school sites to locate in close proximity to commercial areas.
- the joint use of municipal facilities with school boards.

A COMPARISON OF POPULATION AND DENSITY



- 28. seniors housing and child care to locate in close proximity to school sites.
- 29. the integration of public agriculture into school site planning.

Promote the incorporation of walkability and areas for social interaction by **encouraging**:

- 30. new schools be placed in close proximity to new, existing or planned transit corridors.
- 31. new school sites:
 - a. be located in a central area of a neighbourhood or community;
 - b. avoid barriers to accessibility including railroads and major streets, and where barriers cannot be avoided, use mitigation measures to enhance accessibility;
 - c. front onto at least two streets;
 - d. be connected to active transportation infrastructure;
 - e. implement traffic calming at adjacent crossings;
 - f. be accessible from all sides;
 - g. be highly visible;
 - h. contain ample bike parking;
 - i. consider potential expansion areas; and
 - j. offer separate traffic lanes.



Local Community Services

Promote compact, mixed-use development by **encouraging**:

- 32. local community services to locate within, or in close proximity to a mixture of uses including commercial, residential, schools or major community services.

Promote the evolution of complete communities by **encouraging**:

- 33. the integration of public agriculture in conjunction with local community services.

- 34. a variety of public and private local community services.
- 35. new community and seniors housing developments to locate in close proximity to new, existing or planned:
 - a. transit controlled locations;
 - b. major public services which include major community services; and
 - c. community commercial.



Open Space

Ensure the incorporation of walkability and areas for social interaction by **requiring**:

- 36. centrally located public open space.
- 37. that a Park Master Plan be prepared for regional parks and school sites.
- 38. connect active transportation infrastructure to open space through high and medium density residential, mixed-use developments and commercial as part of new development or redevelopment.
- 39. enhancements to open space in conjunction with redevelopments.
- 40. the provision of outdoor amenity spaces for large developments.

Promote the incorporation of walkability and areas for social interaction by **encouraging**:

- 41. public spaces to have the ability to convert uses throughout all four seasons.
- 42. that compact development areas be prioritized as locations for farmers markets and community gardens within Sherwood Park.
- 43. large redevelopment and infill sites to include a central park or plaza.

- 44. that all residences are within close proximity to a park.
- 45. regional parks or parks of a scale and size containing activities that serve a large population base be located in close proximity new high school sites.

Promote the integration of green building and green infrastructure by **encouraging**:

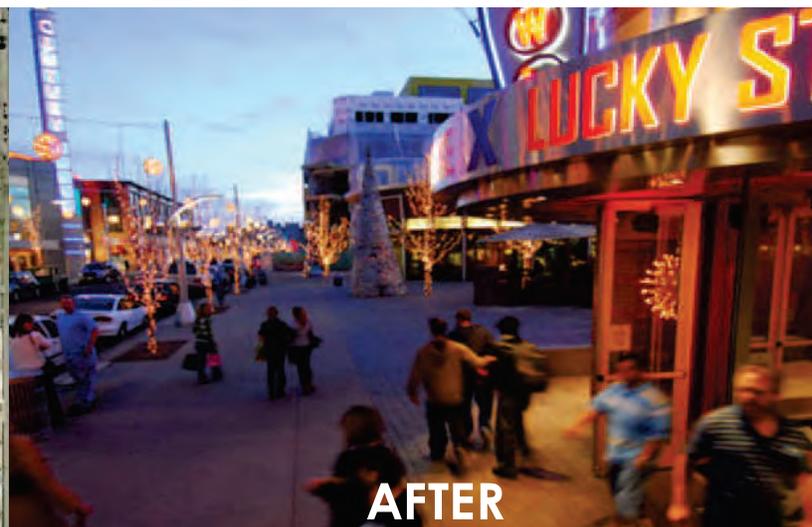
- 46. public agriculture and edible landscapes through integration with landscaping standards.



Transportation

Ensure the incorporation of walkability and areas for social interaction by **requiring**:

- 47. that transportation plans utilize:
 - a. transit oriented development concepts;
 - b. context approach to street design;
 - c. complete street concepts where dictated by the land use context; and
 - d. modal split.
- 48. the connection of active transportation infrastructure and public transportation within the Urban Centre Policy Area and to adjacent policy areas.
- 49. safe, efficient, unobstructed and accessible active transportation infrastructure connections:
 - a. across major streets to provide active transportation access to services;
 - b. between residential and services;
 - c. on both sides of the street; and
 - d. to transit controlled locations.
- 50. that the design and location of crosswalks prioritize the safety and comfort of pedestrians by applying factors such as land use, pedestrian demand, vehicular speed, street width and crash history.
- 51. pedestrian safety measures such as pedestrian islands, raised intersections and curb extensions be utilized.
- 52. pedestrian infrastructure along active transportation infrastructure and at transit controlled locations.
- 53. services to be accessible by active transportation and public transportation at multiple location points.
- 54. redevelopment of large sites to incorporate a grid or modified grid street pattern.
- 55. bicycle parking for all developments.



56. new vehicular parking to consist of one or more of the following:
- on-street;
 - underground; or
 - as stacked parking.

Promote the incorporation of walkability and areas for social interaction by **encouraging**:

57. express transit service from the Compact Development Policy Area to broader destination areas.
58. an on street, at grade, transit transfer facility to locate within Emerald Hills in close proximity to services with the potential for transit priority corridors and higher order transit service in the long term.
59. a reduction in vehicular parking.
60. midblock crosswalks in combination with pedestrian safety measures such as lower vehicular speed, pedestrian islands, raised intersections, curb-extensions, overhead signage, trees, landscaping and stop line setbacks.

Support the incorporation of walkability and areas for social interaction by **considering**:

61. local transit service that:
- serves areas with a high demand for short trips; and
 - provides extra capacity where dense residential areas are close to major employment or education centers
62. surface parking lots where:
- existing surface parking remains following infill of a portion of the lot;
 - it is set back from the street at the rear of buildings or interior of sites and be designed in smaller clusters, separated by landscaped areas;
 - it incorporate pedestrian infrastructure as well as safe and efficient active transportation infrastructure onsite; and
 - it connects active transportation infrastructure safely and efficiently through the site and to transit controlled locations.

